

**GENERAL CONTRACT NOTES (CONT.)**

- 17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING HIS/HER OWN PROJECT OFFICE AND COMPLETE UTILITY SERVICES. THE AIRPORT WILL NOT PROVIDE FACILITIES AND SERVICES TO THE CONTRACTOR DURING CONSTRUCTION. THE CONTRACTOR'S PERSONNEL ARE PROHIBITED FROM UTILIZING THE AIRPORT TERMINALS OR ANY TENANT FACILITIES.
- 18. THE CONTRACTOR SHALL VIDEO TAPE THE ENTIRE WORK AREA AFTER THE PRE-CONSTRUCTION MEETING AND PRIOR TO MOBILIZATION OF PERSONNEL AND EQUIPMENT. IN AREAS WHERE CONSTRUCTION EQUIPMENT CROSSES EXISTING PAVEMENTS, THE CONTRACTOR SHALL TAKE PICTURES OR VIDEO THE PAVEMENT PRIOR TO COMMENCING OPERATIONS. THE CONTRACTOR SHALL INCLUDE TWO COPIES OF ALL VIDEO AND/OR PHOTOGRAPHS TO THE CONSTRUCTION MANAGER AT THE ATTACHMENTS FOR THE FIRST INVOICE REQUEST (ONE FOR THE ENGINEER AND THE OTHER ONE FOR THE AIRPORT). THIS DOCUMENTATION SHALL BE USED TO DETERMINE THE AMOUNT OF DAMAGE, IF ANY, CAUSED TO THE PAVEMENTS AND SURROUNDING FACILITIES BY THE CONSTRUCTION EQUIPMENT AND THE QUALITY OF CONSTRUCTION WHICH SHALL BE REQUIRED FOR THE REPAIRS, IF ANY.
- 19. ALL SAWCUTTING SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS IN THE CONTRACT.
- 20. WEEKLY PROGRESS MEETINGS ARE MANDATORY. THE DAY OF THE WEEK FOR THESE MEETING WILL BE DETERMINED BY AGREEMENT BETWEEN THE AIRPORT MANAGEMENT, THE CONSTRUCTION MANAGER AND THE CONTRACTOR. AT A MINIMUM, THE MEETING WILL BE ATTENDED BY THE PROJECT SUPERINTENDENT, CONSTRUCTING SUPERINTENDENT, FOREMEN OF ACTIVE WORK, (INCLUDING SUBS) AND THE CONTRACTOR'S QC REPRESENTATIVE.
- 21. THE CONTRACTOR SHALL SUBMIT HIS/HER CONSTRUCTION WORK SCHEDULE TO THE CONSTRUCTION MANAGER AND THE RESIDENT PROJECT REPRESENTATIVE (RPR) EACH WEEK PROJECTING UPCOMING WORK FOR THE NEXT THREE WEEKS.
- 22. THE CONTRACTOR'S PROJECT SUPERINTENDENT SHALL PARTICIPATE IN ALL COORDINATION MEETINGS AND SHALL BE ON SITE DURING ALL ACTIVITIES.
- 23. HAUL TRUCKS TRANSPORTING LOOSE MATERIALS SHALL USE LOAD COVERS AND SHALL BE LOADED SUCH THAT NO SPILLAGE OCCURS DURING TRANSIT ON THE STATE, MUNICIPAL, OR AIRPORT ROADWAYS, TAXIWAYS OR APRONS. ALL CONTRACTOR EQUIPMENT MUST BE MAINTAINED IN GOOD WORKING CONDITION WITHOUT ANY LEAKING. CONTRACTOR VEHICLES THAT LEAK OR DRIP OIL AND/OR FLUIDS ONTO ANY APRON, RAMP, TAXIWAYS, RUNWAYS OR ANY STREET PAVEMENT MUST BE REMOVED FROM THE PROJECT FOR REPAIR AT NO COSTS TO THE OWNER.
- 24. THE CONTRACTOR SHALL USE THE AVIATION DEPARTMENT SOIL MANAGEMENT PLAN AT NO ADDITIONAL COST TO THE CITY TO HANDLE ALL CONSTRUCTION ACTIVITIES FOR THE AIRPORT PROJECTS. THE SOIL MANAGEMENT PLAN CAN BE DOWNLOADED FROM THE AVIATION WEBSITE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITIES TO COORDINATE ALL OFF-AIRPORT ROUTES WITH APPROPRIATE AUTHORITIES. PROVIDE COPIES OF ALL APPROVALS TO THE OWNER. ON-AIRPORT HAULING SHALL BE COORDINATED WITH CM AND APPROVED IN ADVANCE TO MINIMIZE IMPACT TO AIRPORT.
- 25. CONTRACTOR EQUIPMENT AND VEHICLES SHALL TRAVEL A MINIMUM AMOUNT ON NEWLY CONSTRUCTED PAVEMENTS SO THAT THESE AREAS WILL NOT BE DAMAGED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO THE NEWLY CONSTRUCTED PAVEMENTS AT THE CONTRACTOR'S EXPENSE. DAMAGE TO EXISTING PAVEMENT OR PAVEMENT MARKING SHALL BE REPAIRED AT ORIGINAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE.
- 26. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE EFFORT AND COST OF IMMEDIATE CLEANING OF EARTH TRACKING AND SPILLS ON PAVED SURFACES RESULTING FROM THE CONTRACTOR'S OPERATIONS. BECAUSE OF THE POTENTIAL FOR EXTREME DAMAGE TO AIRCRAFT FENCES BY INGESTION OF FOREIGN OBJECTS, THE CONTRACTOR SHALL OPERATE AND MAINTAIN MECHANICAL VACUUM SWEEPER (WET/DRY) EQUIPMENT COMPLETE WITH OPERATIONS ON THE PROJECT DURING ANY HAULING OR OTHER OPERATIONS ACROSS TAXIWAYS, TAXILANES OR APRONS. AT A MINIMUM, TWO VAC TRUCKS PER EACH PROJECT ARE REQUIRED, IF THE PROJECT COSTS ARE OVER 5 MILLION DOLLARS, ONE VAC PER CROSSING OF ACTIVE TAXIWAYS, RUNWAYS, PERIMETER ROAD STREETS, AND/OR RAMPS IS REQUIRED. THE VAC OPERATORS CANNOT HAVE OTHER DUTY AT THE SAME TIME.
- 27. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMMEDIATE REPAIR OF ANY DAMAGE TO AIRPORT/AIRLINE FACILITIES, INCLUDING BUT NOT LIMITED TO EXISTING PAVEMENTS, UNDERGROUND CABLES, LIGHTS, SIGNS, BUILDINGS, FENCES, EQUIPMENTS, ETC., CAUSED DURING CONSTRUCTION. ALL REPAIRS MUST BE MADE IN ACCORDANCE WITH THE CM AND EQUAL OR BETTER QUALITY AND IN COMPLIANCE WITH AIRPORT AND/OR FAA REQUIREMENTS. ALL REPAIR AND REPLACEMENT COSTS SHALL BE AT THE EXPENSE OF THE CONTRACTOR.
- 28. ALL MATERIAL SUBMITTALS FOR ITEMS TO BE USED IN CONSTRUCTION OF THE PROJECT SHALL BE SUBMITTED TO THE CONSTRUCTION MANAGER FOR REVIEW AND APPROVAL. A MINIMUM OF 10 DAYS PRIOR TO COMMENCEMENT OF THE ASSOCIATED WORK.
- 29. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL AND DISPOSAL OF ALL WASTE MATERIAL GENERATED DURING CONSTRUCTION. WASTE MATERIAL MUST BE REMOVED FROM THE WORK SITE AND DISPOSED OF IN SUCH A MANNER AS TO NOT DAMAGE THE OWNER'S OR OTHER PERSON'S PROPERTY. (NO SEPARATE PAY FOR THE ITEM).
- 30. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING REQUIRED SECURITY TO PROTECT HIS OWN PROPERTY, EQUIPMENT, AND WORK IN PROGRESS.
- 31. VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD BEFORE COMMENCING ANY WORK. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPORT ANY DISCREPANCIES TO THE OWNER IN A TIMELY MANNER.
- 32. THE DRAWINGS SHOW AS MUCH INFORMATION AS CAN BE REASONABLY OBTAINED FROM THE FIELD OBSERVATION, SURVEY AND EXISTING CONSTRUCTION DRAWINGS REGARDING THE TOPOGRAPHIC FEATURES, AND ELEVATIONS, AS WELL AS THE LOCATION AND NATURE OF PIPELINES, UNDERGROUND CABLES, UTILITIES, ETC., HOWEVER, THE ACCURACY OF COMPLETENESS OF SUCH INFORMATION IS NOT GUARANTEED.
- 33. NO OPEN TRENCHES OR DROP OFFS ARE PERMITTED, AREA NEEDS TO BE FENCED OFF WITH REFLECTIVE SAFETY FENCE, LIGHTED LOW PROFILE BARRICADES AND/OR STEEL PLATES MAY NEED TO BE USED AT NO COSTS TO THE OWNER.
- 34. HORIZONTAL CONTROL MONUMENTS ARE SHOWN ON THE PLANS. CONTRACTOR IS RESPONSIBLE FOR PROVIDING CONSTRUCTION LAYOUTS, AND GRADE CONTROL SURVEYING FOR CONSTRUCTION OF THE PROJECT.
- 35. CONTRACTOR SHALL BE RESPONSIBLE FOR PERMANENT SERVICE TERMINATION ASSOCIATED WITH UTILITY LINES TO BE REMOVED AND ABANDONED.
- 36. CONTRACTOR SHALL MAINTAIN A COPY OF ALL PERMITS ON THE JOBSITE.
- 37. ALL EXCAVATION IS UNCLASSIFIED AND SHALL INCLUDE ROCK AND ALL OTHER MATERIALS ENCOUNTERED REGARDLESS OF THEIR NATURE OR THE MANNER IN WHICH THEY ARE REMOVED.
- 38. WHERE FINISHED CONTOURS ARE SHOWN TO MATCH EXISTING CONTOURS ON THE GRADING PLANS. NO GRADE SEPARATION IS ALLOWABLE. CONTRACTOR SHALL ADJUST GRADING AS NECESSARY TO ACCOMPLISH THIS REQUIREMENT.
- 39. CONTRACTOR AND/OR CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR STRUCTURAL DESIGN/GEOTECHNICAL/SAFETY/EQUIPMENT CONSULTANT, IF ANY, SHALL REVIEW THESE PLANS AND AVAILABLE GEOTECHNICAL INFORMATION AND THE ANTICIPATED INSTALLATION SITE(S) WITHIN THE PROJECT WORK AREA IN ORDER TO IMPLEMENT CONTRACTOR'S TRENCH EXCAVATION SAFETY PROTECTION SYSTEMS, PROGRAMS AND/OR PROCEDURES FOR THE PROJECT DESCRIBED IN THE CONTRACTOR DOCUMENTS.

- 39. (CONT.) THE CONTRACTORS IMPLEMENTATION OF THESE SYSTEMS, PROGRAMS AND/OR PROCEDURES SHALL PROVIDE ADEQUATE TRENCH EXCAVATION SAFETY PROTECTION THAT COMPLY WITH AS A MINIMUM, OSHA STANDARDS FOR TRENCH EXCAVATIONS. SPECIFICALLY CONTRACTOR AND/OR CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR SAFETY CONSULTANT SHALL IMPLEMENT A TRENCH SAFETY PROGRAM IN ACCORDANCE WITH OSHA STANDARDS COVERING THE PRESENCE AND ACTIVITY OF INDIVIDUALS WORKING IN AND AROUND TRENCH EXCAVATION.
- 40. CONTRACTOR SHALL FURNISH THE ENGINEER WITH AS-BUILT PLANS INDICATING THE ACTUAL MEASUREMENTS, LOCATIONS OF UTILITY LINES AND SITE IMPROVEMENTS, INCLUDING FAA SYSTEMS, INSTALLED OR ENCOUNTERED. THE AS-BUILT DRAWINGS HAVE TO BE PROVIDED TO THE ENGINEER WITHIN TWO WEEKS FROM THE FINAL ACCEPTANCE OF THE CONSTRUCTIONS.
- 41. CONTRACTOR SHALL MAINTAIN SERVICE TO EXISTING SANITARY SEWERS AT ALL TIMES DURING CONSTRUCTION.
- 42. DUE TO FEDERAL REGULATIONS TITLE 49, PART 192.181, CITY PUBLIC SERVICE ENERGY MUST MAINTAIN ACCESS TO GAS VALES AT ALL TIMES. THE CONTRACTOR MUST PROTECT AND WORK AROUND GAS VALVES THAT ARE IN THE PROJECT AREAS.
- 43. NO EXTRA PAYMENT WILL BE MADE FOR INCIDENTAL WORK OR MATERIALS REQUIRED TO COMPLETE A CONTRACT PAY ITEM. THIS INCIDENTAL WORK WILL BE REQUIRED AND SHALL BE INCLUDED UNDER THE PAY ITEM TO WHICH IT RELATES.
- 44. WHENEVER POWER POLES ARE ADJACENT TO THE PROPOSED CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE PROPER SHORING, BRACING, AND/OR OTHER SUITABLE SUPPORT FOR THE POLES DURING CONSTRUCTION (NO SEPARATE PAY ITEM).
- 45. OVERHEAD POWER LINES EXIST IN THE AREA OF THE PROJECT OR HAUL ROUTES. TEXAS LAW ARTICLE 1436C, PROHIBITS ALL ACTIVITIES IN WHICH PERSONS OR EQUIPMENT MAY COME WITHIN SIX (6) FEET OF ENERGIZED OVERHEAD POWER LINES, AND FEDERAL REGULATIONS, TITLE 29, PART 1910.180(I) AND PART (1926.550(A) (15) REQUIRE A MINIMUM OF TEN (10) FEET FROM THESE FACILITIES WHERE CONTRACTOR MUST WORK NEAR OVERHEAD POWER LINES, CALL 210-353-2012 FOR THE LINES TO BE DE-ENERGIZED AND/OR MOVED AT CONTRACTOR'S EXPENSE. REGARDING THE NOTIFICATION, THE CONTRACTOR NEEDS TO PROVIDE A TECH OPS AND THE AVIATION DEPARTMENT AIRFIELD FACILITIES SEVEN (7) BUSINESS DAYS IN ADVANCE, AND 1.5 MONTHS FOR CPS ENERGY.
- 46. BLASTING IS NOT ALLOWED.
- 47. WATER LINE CROSSING: WHERE THE MINIMUM 9 FEET SEPARATION DISTANCE BETWEEN SEWER LINES AND WATER LINES/MAIN CANNOT BE MAINTAINED, INSTALLATION OF WATER AND SEWER LINE SHALL BE IN STRICT ACCORDANCE WITH THE TEXAS COMMISSION ON ENVIRONMENTAL QUALITY (30 TAC 290.44(E) AND 317.13 APPENDIX E.)
- 48. EROSION AND SEDIMENTATION CONTROL: STORMWATER POLLUTION PREVENTION PLANS AND PROCEDURES SHALL BE IMPLEMENTED ACCORDING TO TCEQ AND EPA REGULATIONS FOR STORMWATER DISCHARGE FROM CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL MEET ALL REQUIREMENTS OF THE ATTACHED SW3P PLAN AND THESE REGULATIONS.FINAL PROJECT ACCEPTANCE SHALL NOT BE GRANTED UNTIL ALL PERMANENT STABILIZATION MEASURES HAVE BEEN ESTABLISHED. FURTHER INFORMATION REGARDING SAN ANTONIO AIRPORT SYSTEM'S SW3P, SOIL MANAGEMENT PLAN, F FUEL SPILL RESPONSE PLAN, AND NOISE MANAGEMENT PROGRAM CAN BE FOUND AT AVIATION DEPARTMENT WEBSITE UNDER ENVIRONMENTAL STEWARDSHIP: [HTTP://WWW.SANANTONIO.GOV/AVIATION/](http://www.sanantonio.gov/aviation/).
- 49. CONSTRUCTION PERSONNEL WILL NOT BE ALLOWED TO OPERATE VEHICLES OR EQUIPMENT IN THE AOA UNTIL THEY HAVE SUCCESSFULLY COMPLETED THE AIRPORT AOA DRIVING SAFETY COURSES.
- 50. DURING THE PERFORMANCE OF THIS CONTRACT, THE AIRPORT RUNWAYS, TAXIWAYS, AND AIRCRAFT PARKING APRONS SHALL REMAIN IN USE BY AIRCRAFT TO THE MAXIMUM EXTENT POSSIBLE OR AS OTHERWISE NOTED IN THE PLANS. AIRCRAFT USE OF AREAS NEAR THE CONTRACTOR'S WORK AREA WILL BE CONTROLLED BY ATCT GROUND CONTROL, AIRPORT OPERATIONS AND THE AIRLINES TO MINIMIZE DISTURBANCE TO THE CONTRACTOR'S OPERATION. THE CONTRACTOR SHALL NOT ALLOW HIS/HER EMPLOYEES, SUBCONTRACTORS, AND SUPPLIERS TO ENTER OR TO REMAIN IN ANY PART OF THE AIRPORT WHICH COULD BE HAZARDOUS TO PERSONS OR AIRCRAFT OPERATIONS. WHENEVER AIRCRAFT OPERATIONS REQUIRE, THE CONSTRUCTION MANAGER MAY ORDER THE CONTRACTOR TO SUSPEND OPERATIONS, MOVE PLANT, PERSONNEL, EQUIPMENT, AND MATERIALS TO A SAFE LOCATION AND STAND BY UNTIL AIRCRAFT USE IS COMPLETED.
- 51. THE CONTRACTOR SHALL BE RESPONSIBLE TO INSTALL SHEETING AND BRACING AS NECESSARY TO SUPPORT THE SIDES OF ALL TRENCHES AND OTHER EXCAVATIONS AS REQUIRED BY CURRENT OSHA REGULATIONS. COSTS OF SHEETING AND BRACING SHALL BE SUBSIDIARY TO THE CONSTRUCTION.
- 52. THE CONTRACTOR SHALL PROVIDE ONE OR MORE MOTORIZED MECHANICAL VACUUM SWEEPER PLUS AT LEAST ONE OPERATIONAL SPARE, ALONG WITH A "FOREIGN OBJECTS DAMAGE" (FOD) PLAN PRIOR TO BEGINNING WORK. THE FOD PLAN AND DAILY ACTIVITIES SHALL BE PRESENTED BY THE CONTRACTOR PRIOR TO INITIATION OF WORK. THE FOD PLAN IS SUBJECT TO ADVANCE APPROVAL BY THE CONSTRUCTION MANAGER. THE MECHANICAL SWEEPER/VACUUM WILL REMAIN ON SITE AND OPERATIONAL FULL TIME AS LONG AS THAT WORK CONTINUES INSIDE AOA AREA, INCLUDING PERIMETER ROADS, VEHICLE LANES, TENANT ENTRANCE CROSSINGS, TERMINAL RAMPS, TAXIWAYS AND RUNWAYS.
- 53. WORK SHALL NOT COMMENCE UNTIL:
  - A. SUFFICIENT LIGHTED BARRICADES ARE IN PLACE TO CONFINE THE WORK AREA AND TO CREATE A BARRIER TO SEPARATE THE CONSTRUCTION AREA FROM THE NON-CONSTRUCTION AREAS.
  - B. ALL SAFETY EQUIPMENT FOR PERSONNEL AND CONSTRUCTION EQUIPMENT IS IN PLACE AND OPERABLE.
  - C. ALL PERSONNEL WORKING WITH THE AOA HAVE BEEN PROPERLY TRAINED AND BADGED BY THE AIRPORT SECURITY DIVISION AND OPERATIONS DIVISION TO WORK WITHIN THE AOA.
  - D. THE CONTRACTOR HAS FAMILIARIZED HIS/HER PERSONNEL WITH THE HORIZONTAL AND VERTICAL CLEARANCE NEEDED TO PROVIDE FOR SAFE CONSTRUCTION OPERATIONS ADJACENT TO ACTIVE TAXIWAYS AND TAXILANES.
  - E. THE CONTRACTOR HAS TO DEVELOP, SUBMIT HIS/HER QUALITY CONTROL, SITE SAFETY AND FOD PLANS TO THE OWNER FOR APPROVAL.
- 54. THE CONTRACTOR SHALL PROVIDE A FULL TIME SAFETY PROFESSIONAL TO MANAGE THE CONTRACTOR'S SAFETY PLAN.
- 55. THE MAXIMUM HEIGHT OF CONSTRUCTION EQUIPMENT SHALL NOT EXCEED 25' WITHOUT APPROVAL IN ADVANCE.
- 56. ALL CONSTRUCTION EQUIPMENT MUST BE KEPT AT A DISTANCE GREATER THAN 25' FROM THE WINGTIPS OF AIRCRAFT.
- 57. THE CONTRACTOR SHALL MAKE PROVISIONS SUCH THAT HINDRANCE TO SERVICING OPERATIONS AT THE AIRCRAFT GATES ADJACENT TO CONSTRUCTION ARE KEPT TO A MINIMUM.
- 58. ANY FINE ASSESSED TO THE SAN ANTONIO AVIATION DEPARTMENT DUE TO VIOLATION BY THE CONTRACTOR OF FAA SECURITY REQUIREMENTS AND/OR TSA SECURITY REQUIREMENTS WILL BE PASSED ON TO THE CONTRACTOR.
- 59. ALL ACCESS ROADS TO MARSHALLING AREAS AND/OR WORK AREAS ARE TO BE DEVELOPED AND/OR MAINTAINED BY THE CONTRACTOR. THESE ROADS ARE TO BE RETURNED TO THEIR ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT.
- 60. THE CONTRACTOR SHALL PROVIDE A TRAINED UNIFORMED EMPLOYEE ACCEPTABLE TO THE AIRPORT SECURITY DIVISION, AND TSA, AND A FUNCTIONING TELEPHONE AT EACH AOA GATE AT ALL TIMES THAT SUCH A GATE IS UNLOCKED. ALL PERSONNEL AND VEHICLES REQUIRED ACCESS TO THE AOA SHALL COMPLY WITH ALL REQUIREMENTS OF THE AIRPORT SECURITY AND TSA. THE CONTRACTOR SHALL RECORD ALL NAMES AND VEHICLES ENTERING THE AOA GATE CONTROLLED BY THE CONTRACTOR.
- 61. ALL TEMPORARY MARKING AND SIGNS FOR THE PERIMETER ROAD AND/OR ACCESS ROUTES SHALL MEET AC 150/5340-18 STANDARDS FOR AIRPORT SIGN SYSTEMS AND FEDERAL HIGHWAY ADMINISTRATION PUBLICATION, MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS.

- 62. SIGNS FOR VEHICLE OPERATORS SHALL CONFORM TO THE STANDARDS IN THE FEDERAL HIGHWAY ADMINISTRATION (FHWA) PUBLICATION MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) FOR STREETS AND HIGHWAYS.
- 63. BATCH PLANTS ARE PERMITTED AT THE DESIGNATED LOCATION STATED ON THE PLANS. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL APPLICABLE PERMITS. EACH BATCH PLANT MUST BE LIT WITH AN OBSTRUCTION LIGHT MEETING FAA CRITERIA FOR OBSTRUCTION LIGHTING. CONTRACTOR SHALL SUBMIT HEIGHT AND LOCATION OF BATCH PLANT TO FAA FOR REVIEW VIA FAA 7460 FORM.
- 64. TRAFFIC CONTROL OFFICER SHALL BE LOCATED ON SKYPLACE BLVD. DURING HIGH VOLUME HAULING OPERATIONS. TO BE PAID UNDER ITEM 530.1.

**CONSTRUCTION NOTES**

- 1. SANITARY FACILITIES ARE TO BE SUPPLIED BY THE CONTRACTOR AND CAN ONLY BE LOCATED WITHIN SECURED CONSTRUCTION AREAS.
- 2. ALL ACCESS ROADS TO STORAGE AREAS AND/OR WORK AREAS ARE TO BE DEVELOPED AND MAINTAINED BY THE CONTRACTOR. THESE ROADS ARE TO BE RETURNED TO THEIR ORIGINAL OR BETTER CONDITION BY THE CONTRACTOR UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- 3. THE CONTRACTOR'S JOB SUPERINTENDENT IS TO MEET WITH RESIDENT PROJECT ENGINEER (RPR) PRIOR TO THE START OF EACH WORKING DAY TO COORDINATE DAILY CONSTRUCTION ACTIVITIES.
- 4. THE CONTRACTOR IS TO COORDINATE WITH THE RESIDENT PROJECT REPRESENTATIVE (RPR) AND THE AVIATION DEPARTMENT CONCERNING SAFETY IN ASSURING THAT POWER IS OFF IN THE SYSTEM WHILE THE ELECTRICAL WORK IS UNDERWAY. SPECIFICALLY, THEY ARE TO AGREE ON A CONTROL AND CHECK PROCEDURE TO PREVENT ACCIDENTAL POWER RECONNECTION. THESE AGREED UPON PROCEDURES ARE TO BE DOCUMENTED IN A WRITTEN OUTLINE, SIGNED BY BOTH GROUPS AND A COPY ON FILE WITH THE RESIDENT PROJECT ENGINEER (RPR), AND THE AVIATION DEPARTMENT BEFORE ANY SUCH WORK IS UNDERTAKEN.
- 5. CONTRACTOR SHALL VERIFY THE SIZE OF EXISTING CABLES WHERE SPLICES AND CONNECTORS OCCUR IN ORDER TO PROVIDE THE PROPER SPLICE CONNECTOR KITS.
- 6. AT NO TIME SHALL ANY EXCAVATION AREAS BE LEFT UNATTENDED UNLESS APPROPRIATE MARKING AND B BARRICADING IS EMPLOYED TO SATISFY THE OWNER AT NO ADDITIONAL COSTS TO THE OWNER.
- 7. DEMOLITION AND EXCAVATION SOILS:
  - A. BUILDING MATERIALS, UNCLASSIFIED DEMOLITION SOILS AND FENCING SHALL BE LEGALLY DISPOSED OF OUTSIDE AIRPORT PROPERTY.
  - B. CONTAMINATED SOILS, IF ENCOUNTERED, SHALL BE STOCKPILED AND COVERED WITH 10 MIL VISQUEINO TO PREVENT RAINWATER CONTAMINATION AND UNCONTROLLED VAPOR EMISSIONS. ASK FOR PERMISSION BEFORE REMOVING THE CONTAMINATED MATERIAL OUT OFF THE AIRPORT PROPERTY.
  - C. CONCRETE RUBBLE, CEMENT STABILIZED BASE, REINFORCING STEEL AND ASPHALT SHALL BE LEGALLY DISPOSED OF OUTSIDE AIRPORT PROPERTY.
  - D. UNCONTAMINATED SOILS SHALL BE STOCKPILED WITHIN THE PHASING AREAS, INCLUDING HEIGHT RESTRICTION UNTIL SUCH TIME THAT THEY ARE USED OR REMOVED FROM THE AIRPORT.
- 8. SEE STORM WATER POLLUTION PREVENTION PLAN FOR TURF REQUIREMENTS.
- 9. ALL CONCRETE FOR STRUCTURES, CURBS, RIPRAP, ETC, SHALL CONFORM TO FAA SPECIFICATION P-610.
- 10. ALL CONCRETE AND TAXIWAY CONCRETE SHALL CONFORM TO FAA SPECIFICATION P-501.
- 11. CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF ALL UTILITIES IN THE PROJECT AREA UNDER DEMOLITION AND CONSTRUCTION.
- 12. RESTRICTED AIRPORT AREA MUST REMAIN SECURE AT ALL TIMES. TEMPORARY FENCE ADEQUATE FOR THIS PURPOSE SHALL BE ERECTED PRIOR TO DEMOLITION AND ERECTION OF PERMANENT SECURITY FENCE. NO SEPARATE PAY ITEM FOR TEMPORARY FENCES.
- 13. EMPLOYEE PARKING WILL NOT BE PROVIDED BY OWNER. CONTRACTOR SHALL ARRANGE FOR OFF-SITE PARKING AND TRANSPORTATION TO JOB SITE.
- 14. ALL PERSONNEL OPERATING VEHICLE IN THE AOA MUST HAVE RECEIVED TRAINING AND HAVE A TEXAS DRIVERS LICENSE AND AN AIRPORT DRIVING LICENSE.

**MATERIAL SAFETY DATA SHEETS (MSDS):**

- 1. SUBCONTRACTORS ARE REQUIRED TO BRING COPIES OF MSDS'S FOR ALL HAZARDOUS CHEMICALS BROUGHT INTO THE AIRPORT PROPERTY, PROJECT SITES AND/OR THE STAGING AREAS OUTSIDE AIR OPERATIONS AREA. THESE WILL BE RETAINED BY THE MSDS COORDINATOR AND WILL BE ACCESSIBLE TO ALL EMPLOYEES.
- 2. EACH SUBCONTRACTOR MUST BRING ITS SPECIFIC HAZARDOUS COMMUNICATION PROGRAM AND MSDS' IN A BINDER LABELED WITH THE CONTRACTOR'S NAME AND IDENTIFIED AS A HAZARDOUS COMMUNICATION PROGRAM. UPON LEAVING THE SITE, AND THE REMOVAL OF ALL HAZARDOUS MATERIALS, THE BINDER WILL BE RETURNED TO THE SUBCONTRACTORS INVOLVED.

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**SAN ANTONIO INTERNATIONAL AIRPORT**  
SAN ANTONIO, TEXAS

**PERIMETER ROAD RECONSTRUCTION - PACKAGE 1**

**CONSULTANTS**

**ATKINS**

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REVISIONS		
NO.	DESCRIPTION	DATE

DATE ISSUED: 06-14-16  
REVIEWED BY:  
DRAWN BY:  
DESIGNED BY:

AEP PROJECT NUMBER  
**000-0000-000**  
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SHEET TITLE

**GENERAL CONTRACT NOTES**

SHEET NUMBER  
**C006**

**100% BID SET**



**SAN ANTONIO  
INTERNATIONAL AIRPORT**  
SAN ANTONIO, TEXAS

**PERIMETER ROAD  
RECONSTRUCTION  
- PACKAGE 1**

CONSULTANTS

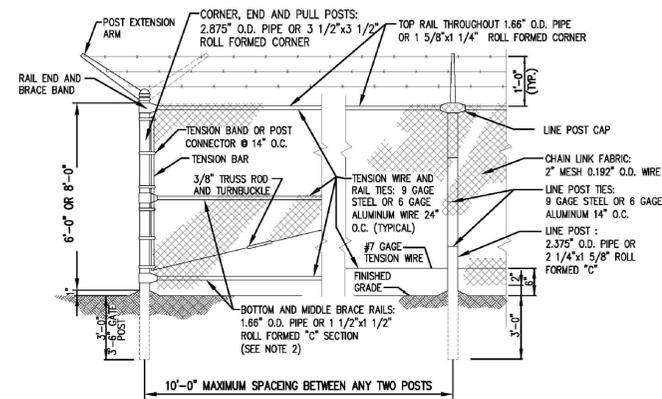
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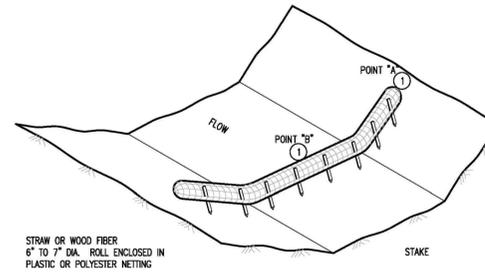


### NOTES

- SUBMIT SHOP DRAWING OF CHAIN LINK FENCE ON TOP OF TEMPORARY PRECAST CONC. BARRIER FOR APPROVAL BY THE ENGINEER. THE SHOP DRAWING SHALL INCLUDE BUT NOT BE LIMITED TO THE METHOD OF SECURING THE FENCE POSTS TO THE CONCRETE BARRIERS.
- REMOVAL OF TEMPORARY FENCE IS INCIDENTAL TO PAY ITEM F-162-5.1.



**1 MODIFIED AOA FENCE**  
SCALE: N.T.S.



STRAW OR WOOD FIBER  
6" TO 7" DIA. ROLL ENCLOSED IN  
PLASTIC OR POLYESTER NETTING

### TYPE 2: BIOROLL DITCH CHECK

USE ON ROUGH GRADED AREAS

#### NOTES:

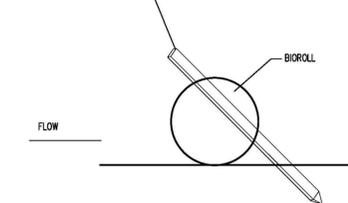
SEE SPECS. FAA 10F P-156

SPACING BETWEEN EACH DITCH CHECK SHOULD BE DETERMINED FROM SPACING FORMULA:

$$\text{SPACING OF DITCH CHECKS (FT.)} = \frac{\text{DITCH CHECK HEIGHT (FT)}}{\% \text{ CHANNEL SLOPE}} \times 100$$

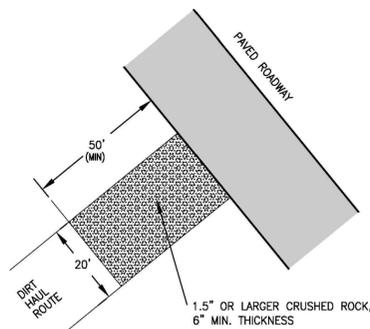
- POINT "A" MUST BE A MINIMUM OF 6 INCHES HIGHER THAN POINT "B" TO ENSURE THAT WATER FLOWS OVER THE DIKE AND NOT AROUND THE ENDS.
- STRAW OR WOOD FIBER WITH GEOTEXTILE FABRIC LINER.

0.5" X 2" X 16" LONG WOODEN STAKES AT  
1' 0" SPACING MAXIMUM. STAKES SHALL BE  
DRIVEN THROUGH THE BACK HALF OF THE  
BIOROLL AT AN ANGLE OF 45 DEGREES WITH  
THE TOP OF THE STAKE POINTING UPSTREAM.



**BIOROLL STAKING DETAIL**

**2 BIOROLL DITCH CHECK**  
SCALE: N.T.S.



**3 CONSTRUCTION ENTRANCE**  
SCALE: N.T.S.

#### NOTES

- ACCESS TO THE SITE SHALL ONLY BE ALONG DESIGNATED HAUL ROUTE.
- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING HAUL ROUTE AND RETURNING SITE TO ORIGINAL CONDITION FOLLOWING COMPLETION OF PROJECT.
- ALL TURF AREAS AFFECTED BY CONSTRUCTION THAT ARE NOT PAVED ARE TO BE TOPSOILED, SEEDED, AND MULCHED.
- THE CRUSHED ROCK CONSTRUCTION ENTRANCE SHALL BE IN PLACE PRIOR TO GRADING.

#### GENERAL EROSION CONTROL NOTES

- EROSION CONTROL SHOWN IS AN EXAMPLE AND MINIMUM. CONTRACTOR SHALL DEVELOP HIS OWN EROSION CONTROL PLAN BASED ON MEANS AND METHODS.
- THE PURPOSE OF EROSION CONTROL IS TO PREVENT POLLUTION OF BODIES OF WATER ON OR ADJACENT TO THE PROJECT SITE. IN ADDITION, EROSION CONTROL SHALL PREVENT DAMAGE TO ADJACENT PROPERTY, AIRPORT PROPERTY AND WORK IN PROGRESS.
- ALL EROSION AND SILTATION MEASURES ARE TO BE PLACED PRIOR TO OR AS THE FIRST STEP IN GRADING.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSPECT ALL EROSION CONTROL DEVICES PERIODICALLY AND AFTER EVERY RAINFALL. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES SHALL BE MADE IMMEDIATELY.
- DURING CONSTRUCTION, ALL STORM DRAIN INLETS WILL BE PROTECTED AS SHOWN, MAINTAINED AND MODIFIED AS REQUIRED BY CONSTRUCTION PROGRESS.
- ALL CONSTRUCTION DISCHARGE WATER SHALL BE ADEQUATELY FILTERED TO REMOVE SILT PRIOR TO DISCHARGE INTO WATERWAYS AND WETLANDS.
- A WATER TRUCK WILL BE KEPT ON SITE DURING EARTHWORK OPERATIONS AND UTILIZED AS REQUIRED TO MINIMIZE DUST FROM WIND AND EARTHWORK OPERATIONS.
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#### REVISIONS

NO.	DESCRIPTION	DATE
1	SHEET REISSUED	6/14/16

DATE ISSUED: 06-14-16

REVIEWED BY:

DRAWN BY:

DESIGNED BY:

AEP PROJECT NUMBER  
**000-0000-000**

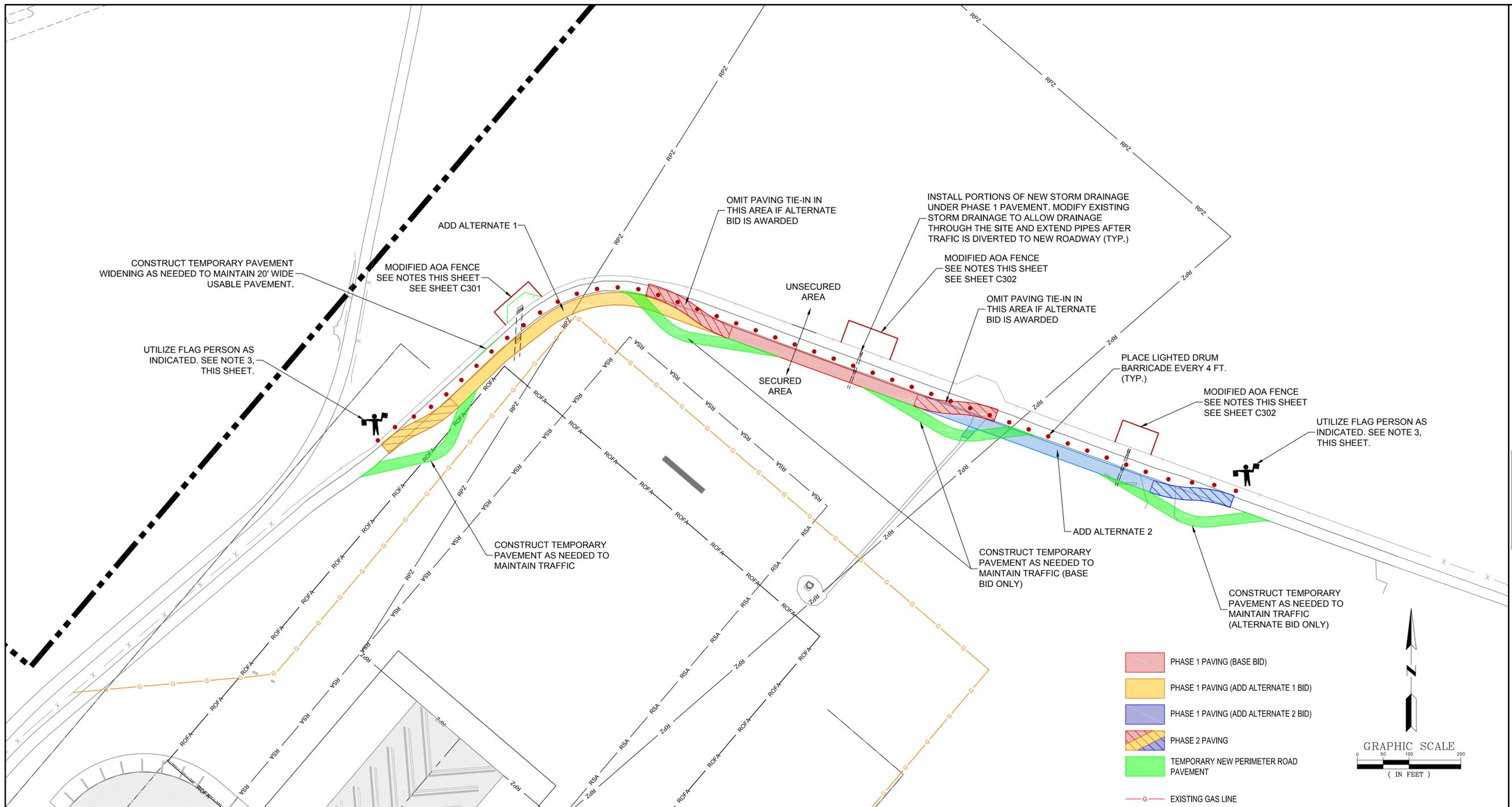
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SHEET TITLE

**FENCE and EROSION  
CONTROL DETAILS**

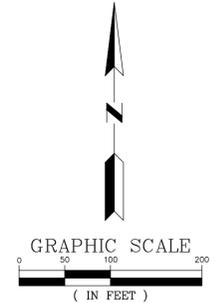
SHEET NUMBER

**C007**

**100%  
BID SET**



- PHASE 1 PAVING (BASE BID)
- PHASE 1 PAVING (ADD ALTERNATE 1 BID)
- PHASE 1 PAVING (ADD ALTERNATE 2 BID)
- PHASE 2 PAVING
- TEMPORARY NEW PERIMETER ROAD PAVEMENT
- G EXISTING GAS LINE



**PHASING NOTES**

1. EXISTING PERIMETER ROAD TO REMAIN OPEN THROUGHOUT CONSTRUCTION.
2. 20' OF USABLE PAVEMENT SHALL BE MAINTAINED AT ALL TIMES THROUGHOUT CONSTRUCTION EXCEPT AS NOTED BELOW.
3. CONTRACTOR MAY PROVIDE FLAG PERSONS IF ONLY ONE LANE OF TRAFFIC IS AVAILABLE BUT FLAG PERSONS SHALL BE PRESENT 24 HRS A DAY FOR THE DURATION OF RESTRICTED TRAFFIC.

**MODIFIED AOA FENCE NOTES**

1. ESTABLISH A TEMPORARY OPENING IN THE EXISTING AOA SECURITY FENCE AS REQUIRED TO SAFELY INSTALL THE UNDERGROUND FACILITY UNDER THE FENCE LINE.
2. PROVIDE AN OFF-DUTY OWNER AUTHORIZED LAW ENFORCEMENT OFFICER (LEO) AT THE FENCE OPENING TO CONTROL ACCESS TO AOA. A LEO SHALL BE IN POSITION 30 MINUTES PRIOR TO ESTABLISHING OPENING TO 30 MINUTES AFTER FENCE OPENING HAS BEEN RE-CLOSED.
3. ESTABLISH A LIMITED ACCESS WORK AREA (LAWA) INSIDE THE AOA. NO WORK OPERATIONS SHALL BE PERFORMED OUTSIDE THE LAWA. ALL CONTRACTOR EMPLOYEES WORKING WITHIN THE LAWA MUST BE BADGED IN ACCORDANCE WITH AIRPORT SECURITY RULES.
4. THE ACTIVE CONSTRUCTION AREA (ACA) SHALL BE LIMITED TO AREA LARGE ENOUGH TO INSTALL UNDERGROUND FACILITIES UNDER THE FENCE TO A POINT WHERE PERMANENT FENCE CAN BE RE-ESTABLISHED.
5. CONTRACTOR SHALL PROSECUTE WORK IN THE ACA CONTINUOUSLY 24/7 UNTIL THE UNDERGROUND FACILITY HAS BEEN INSTALLED TO A POINT WHERE THE FENCE OPENING CAN BE CLOSED AND THE PERMANENT FENCE HAS BEEN RE-ESTABLISHED AND ACCEPTED BY OWNER AND TSA.
6. A TEMPORARY OPENING IS ONLY AUTHORIZED TO ALLOW CONTRACTOR TO ACTIVELY PROSECUTE CONSTRUCTION. IF CONTRACTOR CHOOSES TO DISCONTINUE ACTIVELY PROSECUTING CONSTRUCTION THE OPENING SHALL BE CLOSED.
7. SUBMIT MATERIALS METHODS FOR ESTABLISHING AND CLOSING THE FENCE OPENING.
8. SUBMIT A CONSTRUCTION OPERATIONS PLAN FOR APPROVAL OUTLINING STEP BY STEP PROCEDURES AND SCHEDULE FOR ALL WORK RELATED TO INSTALLING THE UNDERGROUND FACILITY UNDER THE FENCE.

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**PHASING PLAN  
AND PHASING  
NOTES**

SHEET NUMBER  
**C009**

**100%  
BID SET**