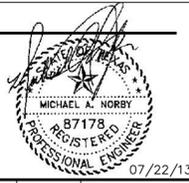




**Kimley-Horn
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601 NW LOOP 410, SUITE 350
SAN ANTONIO, TEXAS 78216
PHONE: (210) 541-9166
TEXAS REGISTERED FIRM,
NO. 928

**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



RUNWAY 12R-30L REHABILITATION AND TERMINAL AREA TAXIWAY IMPROVEMENTS (PACKAGE I)		
SHEET NUMBER	CIVIL NO.	DESCRIPTION
BASE BID: GENERAL		
1	C1.1	COVER SHEET
2	C1.2	SHEET INDEX
3	C1.3	SHEET INDEX
4	C1.4	PROJECT KEYMAP
5	C1.5	ABBREVIATIONS AND LEGEND
6	C1.6	SUMMARY OF QUANTITIES
7	C1.7	SUMMARY OF QUANTITIES
8	C1.8	SUMMARY OF QUANTITIES
9	C1.9	GENERAL NOTES
10	C1.10	GENERAL NOTES
11	C1.11	PROJECT LAYOUT PLAN
12	C1.12	HORIZONTAL AND VERTICAL CONTROL PLAN
BASE BID: OPERATIONS AND PHASING		
13	C2.1	OVERALL PHASING PLAN
14	C2.2	CONSTRUCTION PHASING PLAN
15	C2.3	CONSTRUCTION PHASING PLAN ALT 1 PHASE 1A
16	C2.4	CONSTRUCTION PHASING PLAN ALT 1 PHASE 1B
17	C2.5	CONSTRUCTION PHASING PLAN ALT 2 PHASE 2
18	C2.6	CONSTRUCTION PHASING PLAN ALT 3 PHASE 3
19	C2.7	CONSTRUCTION PHASING PLAN ALT 4 PHASE 4
20	C2.8	CONSTRUCTION PHASING PLAN ALT 5 PHASE 5A & 5B
21	C2.9	PHASING DETAILS
BASE BID: DEMOLITION		
22	C3.1	DEMOLITION PLAN
23	C3.2	DEMOLITION PLAN
24	C3.3	DEMOLITION PLAN
BASE BID: TYPICAL SECTIONS		
25	C4.1	TYPICAL SECTIONS
26	C4.2	TYPICAL SECTIONS
27	C4.3	TYPICAL SECTIONS
BASE BID: GEOMETRICS		
28	C5.1	GEOMETRIC LAYOUT PLAN
29	C5.2	GEOMETRIC LAYOUT PLAN
BASE BID: PLAN AND PROFILES		
30	C6.1	PLAN AND PROFILE RUNWAY 12R END
31	C6.2	PLAN AND PROFILE RUNWAY 30L END
32	C6.3	PLAN AND PROFILE TAXIWAY W
BASE BID: PAVEMENT JOINTS		
33	C7.1	PAVEMENT JOINT LAYOUT PLAN
34	C7.2	PAVEMENT JOINT LAYOUT PLAN
35	C7.3	PAVEMENT JOINT DETAILS
BASE BID: PAVEMENT ELEVATIONS		
36	C8.1	PAVING ELEVATION PLAN
37	C8.2	PAVING ELEVATION PLAN
BASE BID: GRADING AND DRAINAGE		
38	C9.1	GRADING AND DRAINAGE PLAN
39	C9.2	GRADING AND DRAINAGE PLAN
40	C9.3	STORM DRAIN PROFILES
41	C9.4	STORM DRAIN STRUCTURE DETAILS
42	C9.5	STORM DRAIN STRUCTURE DETAILS
43	C9.6	STORM DRAIN STRUCTURE DETAILS
BASE BID: UNDERDRAIN		
44	C10.1	UNDERDRAIN PLAN
45	C10.2	UNDERDRAIN DETAILS
46	C10.3	UNDERDRAIN DETAILS
BASE BID: GROOVING		
47	C11.1	GROOVING PLAN
BASE BID: PAVEMENT MARKING		
48	C12.1	PAVEMENT MARKING PLAN
49	C12.2	PAVEMENT MARKING PLAN
50	C12.3	RUNWAY PAVEMENT MARKING
51	C12.4	PAVEMENT MARKING DETAILS
52	C12.5	PAVEMENT MARKING DETAILS
BASE BID: STORM WATER POLLUTION PREVENTION		
53	C13.1	STORM WATER POLLUTION PREVENTION COVER SHEET
54	C13.2	STORM WATER POLLUTION PREVENTION PLAN AND SEEDING
55	C13.3	STORM WATER POLLUTION PREVENTION PLAN AND SEEDING
56	C13.4	STORM WATER POLLUTION PREVENTION PLAN DETAILS

BID BASE: ELECTRICAL PLANS		
SHEET NUMBER	CIVIL NO.	DESCRIPTION
57	E0.1	ELECTRICAL SCOPE OF WORK
58	E0.2	ELECTRICAL SYMBOL LIST AND GENERAL NOTES
59	E1.1	ELECTRICAL LAYOUT PLAN
60	E1.2	ELECTRICAL LAYOUT PLAN
61	E1.3	ELECTRICAL LAYOUT PLAN
62	E2.1	ELECTRICAL FIXTURE LAYOUT PLAN
63	E2.2	ELECTRICAL FIXTURE LAYOUT PLAN
64	E2.3	ELECTRICAL FIXTURE LAYOUT PLAN
65	E2.4	ELECTRICAL FIXTURE DATA TABLES
66	E3.1	SIGN DETAILS FOR INSTALLATION IN UNPAVED AREAS
67	E3.2	SIGN DETAILS - INSTALLATION IN NEW PCCP
68	E3.3	SIGN & EDGE LIGHT DETAILS - INSTALLATION IN EXISTING PCCP
69	E3.4	ELEVATED TAXIWAY EDGE LIGHT DETAILS
70	E3.5	IN-PAVEMENT LIGHT DETAILS - NEW PCCP
71	E3.6	LIGHT FIXTURE/JOINT COORDINATION DETAILS
72	E3.7	DUCT DETAILS
73	E3.8	HANDHOLE DETAILS
74	E3.9	JUNCTION CAN PLAZA AND HANDHOLE DETAILS
75	E3.10	SUPPLEMENTAL WINDCONE DETAILS
76	E3.11	MISCELLANEOUS DETAILS
77	E3.12	JUNCTION CAN PLAZA AND HANDHOLE DETAIL PLANS
78	E3.13	TEMPORARY EDGE LIGHT SYSTEM DETAILS
79	E4.1	CIRCUIT MAP 12R-30L EDGE (CKT 31)
80	E4.2	CIRCUIT MAP 12R & 30L C/L (CKTS 30 & 32)
81	E4.3	CIRCUIT MAPS 12R TDZ (CKT 33)
82	E4.4	CIRCUIT MAPS LIMA RGL (CKT 10) & LAHSO STOP BAR (CKT 31)
83	E4.5	CIRCUIT MAPS T/W G C/L EAST (CKT 15) & T/W G3 C/L (N/A)
84	E4.6	CIRCUIT MAPS T/W EDGE G3 (CKT 6)
85	E4.7	CIRCUIT MAP T/W EDGE F1 (CKT 57)
86	E5.2	ELECTRICAL SIGN PLAN
87	E6.1	ELECTRICAL DEMOLITION PLAN
88	E6.2	ELECTRICAL DEMOLITION PLAN
89	E6.3	ELECTRICAL DEMOLITION PLAN

ALTERNATE 1		
SHEET NUMBER	CIVIL NO.	DESCRIPTION
ALTERNATE 1: DEMOLITION		
90	C3.4	DEMOLITION PLAN
91	C3.5	DEMOLITION PLAN
ALTERNATE 1: GEOMETRICS		
92	C5.3	GEOMETRIC LAYOUT PLAN
93	C5.4	GEOMETRIC LAYOUT PLAN
ALTERNATE 1: PLAN AND PROFILES		
94	C6.4	PLAN AND PROFILE TAXILANE
95	C6.5	PLAN AND PROFILE TAXILANE
96	C6.6	PLAN AND PROFILE TAXIWAYS G1/G2/G3
ALTERNATE 1: PAVEMENT JOINTS		
97	C7.4	PAVEMENT JOINT LAYOUT PLAN
98	C7.5	PAVEMENT JOINT LAYOUT PLAN
ALTERNATE 1: PAVEMENT ELEVATIONS		
99	C8.3	PAVING ELEVATIONS PLAN
100	C8.4	PAVING ELEVATIONS PLAN
ALTERNATE 1: GRADING AND DRAINAGE		
101	C9.7	GRADING AND DRAINAGE PLAN
102	C9.8	GRADING AND DRAINAGE PLAN
103	C9.9	STORM DRAIN PROFILES
104	C9.10	STORM DRAIN PROFILES
ALTERNATE 1: PAVEMENT MARKING		
105	C12.6	PAVEMENT MARKING PLAN
106	C12.7	PAVEMENT MARKING PLAN
ALTERNATE 1: STORM WATER POLLUTION PREVENTION		
107	C13.5	STORM WATER POLLUTION PREVENTION PLAN AND SEEDING
108	C13.6	STORM WATER POLLUTION PREVENTION PLAN AND SEEDING
ALTERNATE 1: ELECTRICAL PLANS		
109	E0.1	ELECTRICAL SCOPE OF WORK
110	E1.2	ELECTRICAL LAYOUT PLAN
111	E1.3	ELECTRICAL LAYOUT PLAN
112	E1.4	ELECTRICAL LAYOUT PLAN
113	E2.2	ELECTRICAL FIXTURE LAYOUT PLAN
114	E2.3	ELECTRICAL FIXTURE LAYOUT PLAN
115	E2.4	ELECTRICAL FIXTURE DATA TABLES
116	E4.6	CIRCUIT MAPS T/W EDGE G3 (CKT 6)
117	E4.7	CIRCUIT MAP T/W EDGE F1 (CKT 57)
118	E5.1	ELECTRICAL SIGN PLAN
119	E5.2	ELECTRICAL SIGN PLAN
120	E6.2	ELECTRICAL DEMOLITION PLAN
121	E6.3	ELECTRICAL DEMOLITION PLAN
122	E6.4	ELECTRICAL DEMOLITION PLAN

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MARK	DATE	DESCRIPTION
ISSUE:		
PROJECT NO:	33-00178 and 33-00193	
FILE NAME:	_33-00193-R1GN-001	
DRAWN BY:	CAD	
CHECKED BY:	MAN	
COPYRIGHT:	2013	
SHEET TITLE:		

SHEET INDEX

SUMMARY OF QUANTITIES - BID BASE

Item	Specification Number	Bid Item Description	Quantity	Unit
1	100.1	Mobilization/Demobilization	1	LS
2	100.2	Insurance and Bond	1	LS
3	100.3	Airside Safety and Security / Traffic Control	1	LS
4	101.1	Preparing Right-of-Way	1	LS
5	P-100-2.1	Contractor Quality Control	1	LS
6	P-101-5.1	Portland Cement Concrete Pavement Removal, Including Thickened Edge and Reinforcement	8,010	SY
7	P-101-5.3	Cement-Treated Base Removal	8,010	SY
8	P-101-5.4	Concrete Pavement Saw Cut (Full Depth)	1,290	LF
9	P-151-4.1	Clearing and Grubbing	2.75	AC
10	P-151-4.2	Storm Drain Removal	328	LF
11	P-151-4.3	Storm Drain Structure Removal	3	EA
12	P-152-4.1	Unclassified Excavation	8,220	CY
13	P-155-8.1	Lime-Treated Subgrade (6" Depth)	10,180	SY
14	P-155-8.2	Lime	220	Ton
15	Item 540	Storm Water Pollution Prevention Plan	1	LS
16	P-208-5.1	Uncrushed Aggregate Base Course, 13" Depth	1,710	SY
17	P-209-5.1	Crushed Aggregate Base Course, 6" Depth	9,070	SY
18	P-209-5.2	Crushed Aggregate Base Course, 10" Depth	2,430	SY
19	P-304-8.2	Cement-Treated Base Course, 12" Depth	9,070	SY
20	P-403-8.1	Bituminous Pavement (3" Shoulder Surface Course)	1,220	SY
21	P-501-8.1a	Portland Cement Concrete Pavement, 16"	11,120	SY
22	P-604-7.1	Prefomed Sealer, 1/2-inch Joint	8,450	LF
23	P-604-7.2	Prefomed Sealer, 1-inch Joint	600	LF
24	TxDOT 712.3a	Edge Seal (Hot Applied Joint Routing and Sealing)	1,130	LF
25	P-620-5.2	Reflective White Runway Pavement Markings, Waterborne	34,510	SF
26	P-620-5.3	Non-Reflective Black Runway and Taxiway Pavement Markings, Waterborne	4,020	SF
27	P-620-5.7	Pavement Making Obliteration	2,010	SF
28	P-621-5.1	Concrete Pavement Saw Cut Grooving (Runway)	7,060	SY
29	T-901-5.1	Hydro-Mulch Seeding	0.46	AC
30	T-904-5.1	Sodding	1,410.00	SY
31	T-905-5.1	Topsailing	560	CY
32	D-701-5.3	42" RGRCP, Class V	330	LF
33	D-705-5.1	Runway Edge Drain System	1	LS
34	D-751-5.2	Catch Basin	2	EA
35	GP 60-05	Field Office and Curing Facilities	1	LS
36	L-100-5.2	Windcone Relocation	1	LS
37	L-100-5.3	Electrical Demolition	0.20	LS
38	L-100a-3.1	Photometric Testing	1	LS
39	L-108-5.1	L-824, Type C, 1/C #8, 5 kV Cable	10,400	LF
40	L-108-5.2	L-824, Type C, 1/C #6, 5 kV Cable	9,100	LF
41	L-108-5.3	Bare, 1/C #6, Counterpoise Cable	6,000	LF
42	L-110-5.1	Single-way 2" Conduit, Direct Buried	2,350	LF
43	L-110-5.2	Multiple-way Duct, (8) 2-inch Conduit, Direct Buried	980	LF
44	L-110-5.3	Single-way, (1) 2" Conduit, Concrete Encased	2,390	LF
45	L-110-5.4	Multiple-way Duct, (4) 2-inch Conduit, Concrete Encased	10	LF
46	L-110-5.5	Multiple-way Duct, (8) 2-inch Conduit, Concrete Encased	250	LF
47	L115-5.1	New Concrete Handhole, Type II, Furnished & Installed	3	EA
48	L115-5.2	Two-Can Junction Can Plaza, Furnished & Installed	3	EA
49	L-850-4.1	Salvage and Reinstall In-pavement L-850A Runway Centerline Light with New Isolation Transformers on New or Existing Base	21	EA
50	L-850-4.2	Salvage and Reinstall In-pavement L-850B Runway TDZ Light with New Isolation Transformer on New or Existing Base	30	EA
51	L-850-4.3	New In-pavement L-850C Runway Edge Light with New Isolation Transformer on New or Existing Base	1	EA
52	L-858-5.1	New Size 3, 1-Module Airside LED Guidance Sign, Installed on Any Foundation or Base Assembly	1	EA
53	L-858-5.3	New Size 3, 3-Module Airside LED Guidance Sign, Installed on Any Foundation or Base Assembly	1	EA
54	L-858-5.4	New Size 3, 4-Module Airside LED Guidance Sign, Installed on Any Foundation or Base Assembly	1	EA
55	L-861T-4.1	New L-861T(L) LED Taxiway Edge Light with New Isolation Transformer on New or Existing Base	2	EA
56	L-861T-4.3	Salvage and Reinstall L-862 Runway Edge Light with New Isolation Transformer on New or Existing Base	7	EA
57	L-867/868-6.1	Size "B" L-867 Base Can for Any New, Reinstalled or Future Fixture in New Asphalt Shoulder	21	EA
58	L-867/868-6.3	Size "B" L-868 Base Can - "Standard Installation (New PCCP)"	38	EA
59	L-867/868-6.4	Size "B" L-868 Base Can - "Core Drill New PCCP" Installation at Joint or Sawcut	11	EA
60	L-867/868-6.5	Size "B" L-868 Base Can - "Diamond Leave-out" Installation at Joint or Sawcut Intersection	2	EA
61	L-867/868-6.6	New Size "B" L-867 Blank Base Can Cover	12	EA
62	L-867/868-6.8	Concrete Foundation for 1-Module Sign	1	EA
63	L-867/868-6.9	Concrete Foundation for 2-Module Sign	2	EA
64	L-867/868-6.10	Concrete Foundation for 3-Module Sign	2	EA
65	L-867/868-6.11	Concrete Foundation for 4-Module Sign	2	EA
66	SP-XX.X	Contract Allowance	1	Allow
PHASE BASED QUANTITIES				
67	P-620-5.1	Reflective Yellow Taxiway Pavement Markings, Waterborne	275	SF

SUMMARY OF QUANTITIES - ALTERNATE 1

Item	Specification Number	Bid Item Description	Quantity	Unit
1	100.1	Mobilization/Demobilization	1	LS
2	100.2	Insurance and Bond	1	LS
3	100.3	Airside Safety and Security / Traffic Control	1	LS
4	101.1	Preparing Right-of-Way	1	LS
5	P-100-2.1	Contractor Quality Control	1	LS
6	P-101-5.1	Portland Cement Concrete Pavement Removal, Including Thickened Edge and Reinforcement	4,890	SY
7	P-101-5.3	Cement-Treated Base Removal	5,280	SY
8	P-101-5.4	Concrete Pavement Saw Cut (Full Depth)	1,280	LF
9	P-151-4.1	Clearing and Grubbing	3.12	AC
10	P-151-4.2	Storm Drain Removal	759	LF
11	P-151-4.3	Storm Drain Structure Removal	5	EA
12	P-152-4.1	Unclassified Excavation	9,330	CY
13	P-155-8.1	Lime-Treated Subgrade (6" Depth)	14,060	SY
14	P-155-8.2	Lime	300	Ton
15	Item 540	Storm Water Pollution Prevention	1	LS
16	P-208-5.1	Uncrushed Aggregate Base Course, 13" Depth	2,390	SY
17	P-209-5.1	Crushed Aggregate Base Course, 6" Depth	12,450	SY
18	P-304-8.2	Cement-Treated Base Course, 12" Depth	12,450	SY
19	P-403-8.1	Bituminous Pavement (3" Shoulder Surface Course)	1,780	SY
20	P-501-8.1a	Portland Cement Concrete Pavement, 16"	12,220	SY
21	P-604-7.1	Prefomed Sealer, 1/2-inch Joint	8,420	LF
22	P-604-7.2	Prefomed Sealer, 1-inch Joint	1,670	LF
23	TxDOT 712.3a	Edge Seal (Hot Applied Joint Routing and Sealing)	730	LF
24	P-620-5.1	Reflective Yellow Taxiway Pavement Markings, Waterborne	2,470	SF
25	P-620-5.3	Non-Reflective Black Runway and Taxiway Pavement Markings, Waterborne	5,360	SF
26	P-620-5.7	Pavement Making Obliteration	5,360	SF
27	P-621-5.1	Concrete Pavement Saw Cut Grooving (Runway)	7,060	SY
28	T-901-5.1	Hydro-Mulch Seeding	0.16	AC
29	T-904-5.1	Sodding	1,410.00	SY
30	T-905-5.1	Topsailing	590	CY
31	D-701-5.1	24" RGRCP, Class V	45	LF
32	D-701-5.2	30" RGRCP, Class V	45	LF
33	D-701-5.3	42" RGRCP, Class V	720	LF
34	D-701-5.4	Concrete Pipe Collar	1	EA
35	D-751-5.1	Storm Drain Manhole	1	EA
36	D-751-5.2	Catch Basin	4	EA
37	GP 60-05	Field Office and Curing Facilities	1	LS
38	L-100-5.3	Electrical Demolition	0.20	LS
39	L-108-5.1	L-824, Type C, 1/C #8, 5 kV Cable	2,000	LF
40	L-108-5.3	Bare, 1/C #6, Counterpoise Cable	5,200	LF
41	L-110-5.1	Single-way 2" Conduit, Direct Buried	990	LF
42	L-110-5.3	Single-way, (1) 2" Conduit, Concrete Encased	2,240	LF
43	L-110-5.4	Multiple-way Duct, (4) 2-inch Conduit, Concrete Encased	1,050	LF
44	L-110-5.6	Multiple-way Duct, (4) 2-inch HDPE Conduit, Concrete Encased	250	LF
45	L-110-5.8	System Drain, (1) 2" Conduit, Concrete Encased	630	LF
46	L115-5.1	New Concrete Handhole, Type II, Furnished & Installed	4	EA
47	L-867/868-6.1	Size "B" L-867 Base Can for Any New, Reinstalled or Future Fixture in New Asphalt Shoulder	17	EA
48	L-867/868-6.3	Size "B" L-868 Base Can - "Standard Installation (New PCCP)"	29	EA
49	L-867/868-6.4	Size "B" L-868 Base Can - "Core Drill New PCCP" Installation at Joint or Sawcut	1	EA
50	L-867/868-6.6	New Size "B" L-867 Blank Base Can Cover	17	EA
51	L-867/868-6.7	New Size "B" L-868 Blank Base Can Cover	30	EA
52	L-867/868-6.9	Concrete Foundation for 2-Module Sign	2	EA
53	L-867/868-6.10	Concrete Foundation for 3-Module Sign	2	EA
54	L-867/868-6.13	Sign Base Assembly in New PCCP, Any Single Sign Array	1	EA
55	SP-XX.X	Contract Allowance	1	Allow
PHASE BASED QUANTITIES				
56	P-620-5.1	Reflective Yellow Taxiway Pavement Markings, Waterborne	2,000	SF
57	P-620-5.3	Non-Reflective Black Runway and Taxiway Pavement Markings, Waterborne	1,700	SF
58	L-105-6.1	Temporary Jumper, L-824, Type C, 1/C #6, 5 kV Cable in Conduit	100	LF
59	L-105-6.2	Long Term Temporary Direct Buried L-824, Type C, 1/C #6, 5 kV Cable,	100	LF
60	L-105-6.3	Long Term Secondary Circuit, (2)-1/C #10, Temporary	1,010	LF
61	L-105-6.4	Temporary, Surface Mounted Single-way 2" Conduit	20	LF
62	L-105-6.5	Long Term Temporary, Single-way 1-1/4" Conduit	50	LF
63	L-105-6.6	Long Term Temporary Isolation Transformer - Install	3	EA
64	L-105-6.7	Long Term Temporary Stake Mounted Taxiway Edge Light - Install	4	EA
65	L-105-6.9	Long Term Temporary Pavement Mounted Taxiway Edge Light - Install	14	EA



Kimley-Horn and Associates, Inc.

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PHONE: (210) 541-9166

TEXAS REGISTERED FIRM,
NO. 928

RUNWAY 12R-30L REHABILITATION AND TERMINAL AREA TAXIWAY IMPROVEMENTS (PACKAGE 1)



07/22/13

MARK	DATE	DESCRIPTION
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ISSUE: -

PROJECT NO: 33-00178 and 33-00193

FILE NAME: _33-00193-R1GN-002

DRAWN BY: PAC

CHECKED BY: MAN

COPYRIGHT: 2013

SHEET TITLE:

SUMMARY OF QUANTITIES

GENERAL NOTES:

- THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CONTRACT PLANS AND SPECIFICATIONS. THE PROJECT IS SUBJECT TO REVIEW BY REPRESENTATIVES OF THE SAN ANTONIO AIRPORT (AIRPORT), THE PROGRAM MANAGER/CONSTRUCTION MANAGER (PM/CM), THE ENGINEER, THE FEDERAL AVIATION ADMINISTRATION (FAA) AND THE TRANSPORTATION SAFETY AGENCY (TSA). THE CONTRACTOR SHALL PROVIDE UNRESTRICTED ACCESS TO THE SITE FOR INSPECTION PURPOSES DURING THE ENTIRE CONSTRUCTION PERIOD.
- ALL CONTRACTOR PERSONNEL AGENTS, SUBCONTRACTORS, SUPPLIERS, VEHICLES, EQUIPMENT AND MATERIALS SHALL REMAIN WITHIN THE LIMITS OF THE PROJECT AND WITHIN THE SITE ACCESS AND HAUL ROUTES DESIGNATED ON THE PLANS.
- THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION WITH THE CM PRIOR TO COMMENCING WORK.
- THE AIR OPERATIONS AREA (AOA) IS DEFINED AS ALL PAVED AND UNPAVED AREAS OF THE AIRPORT INSIDE THE PERIMETER SECURITY FENCE AND INCLUDES BUT IS NOT LIMITED TO APRONS, TAXIWAYS, RUNWAYS AND RUNWAY/TAXIWAY SAFETY AREAS. NO VEHICLE OR EQUIPMENT SHALL MOVE UPON THE AOA UNLESS THE VEHICLE IS IDENTIFIED BY COMPANY LOGO OR NAME (12" MIN LOGO/6" MIN LETTERS) ON EACH SIDE OF THE VEHICLE. THE APPROPRIATE FLAG OR FLASHING AMBER LIGHT (FLAG OR LIGHT FOR DAYTIME ACTIVITY AND LIGHT FOR NIGHTTIME ACTIVITY) AND IS MONITORING THE APPROPRIATE RADIO FREQUENCY OR IS UNDER THE ESCORT OF A VEHICLE MONITORING THE APPROPRIATE RADIO FREQUENCY. THE CONTRACTOR SHALL PROVIDE THE NECESSARY NUMBER OF RADIOS FOR THE VEHICLES AND EQUIPMENT.
- MONITOR SAN ANTONIO GROUND CONTROL FOR 121.9 OR AS ASSIGNED BY THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES.
- FLAG MEN OR ESCORTS WILL BE REQUIRED TO DIRECT THE CONTRACTOR'S TRUCKS AND EQUIPMENT WHICH ARE OPERATING ON ACTIVE AREAS OF THE AOA IF NOT IN RADIO CONTACT WITH THE TOWER.
- IT IS THE INTENT OF THESE PLANS TO MINIMIZE INTERFERENCE TO AIRCRAFT MOVEMENTS. THEREFORE, IN AREAS OF THE AOA, AIRCRAFT MOVEMENT SHALL HAVE THE RIGHT-OF-WAY.
- CONTRACTOR SHALL OBTAIN ALL DEVELOPMENT AND CONSTRUCTION PERMITS REQUIRED AT HIS/HER EXPENSE PRIOR TO COMMENCEMENT OF THE WORK.
- THE PROJECT PAY ITEMS ARE PROVIDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL WORK NOT IDENTIFIED WITH A SPECIFIC PAY ITEM IS TO BE CONSIDERED REQUIRED/INCIDENTAL WORK TO COMPLETE THE PROJECT AND IS TO BE INCLUDED IN THE COST OF THE PROJECT PAY ITEMS PROVIDED.
- THE CONTRACTOR SHALL HAVE SUFFICIENT EQUIPMENT AND PERSONNEL ON SITE TO ACCOMPLISH EFFICIENT AND PROMPT CONSTRUCTION OF THE VARIOUS WORK ITEMS, INCLUDING WORK ON MORE THAN ONE WORK ITEM SIMULTANEOUSLY.
- THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AND LOCATE ALL SERVICES A MINIMUM OF 48 HOURS PRIOR TO BEGINNING ANY WORK IN THE AREA. IN THE EVENT OF DAMAGE TO UNDERGROUND UTILITIES, AIRPORT OR FAA POWER OR COMMUNICATION LINES, WHETHER SHOWN ON THE DRAWINGS OR NOT, THE CONTRACTOR SHALL MAKE AND EXPEDITE REPAIRS TO REPLACE THE UTILITY LINE BACK IN FULL SERVICE AT NO INCREASE IN COST TO THE PROJECT.

EXISTING UNDERGROUND UTILITIES ARE SHOWN FROM AVAILABLE UTILITY RECORDS AND OBSERVABLE SURFACE FEATURES. ACTUAL LOCATIONS MAY VARY AND UTILITIES NOT SHOWN ON THESE PLANS MAY EXIST. CONTRACTOR SHALL COORDINATE WITH LOCAL UTILITY COMPANIES AND AIRPORT MAINTENANCE PERSONNEL FOR ASSISTANCE IN LOCATING ALL UNDERGROUND FACILITIES IN THE PROJECT AREA PRIOR TO CONSTRUCTION.

UTILITY SERVICE	CONTACT	PHONE NUMBER
SAN ANTONIO WATER SYSTEMS	GOVERNMENTAL RELOCATION	(210) 233-3466 (210) 233-3705
CPS ENERGY (CITY OF SAN ANTONIO PROJECTS)	JOHN OFFER	(210) 353-2012
CPS ENERGY (PROJECTS OTHER THAN CITY OF SAN ANTONIO PROJECTS)	RICHARD RODRIGUEZ	(210) 353-2226
ATT	SANTIAGO PRINCE, LONG DISTANCE THOMAS HARPER, LOCAL	(210) 471-0022 (210) 283-1990
TIME-WARNER	STEPHON ROBERTSON	(210) 382-4306
MCI/VERIZON	JERRY REEVES	(214) 674-0555
GREY FOREST GAS	VANESSA LOPEZ	(210) 695-5992
BEXAR METROPOLITAN WATER DISTRICT	LEONARD MARTIN	(210) 354-6537
GRANDE COMMUNICATION	GABRIEL MORALES	(210) 389-6011
VIA	ERNEST SWEET	(210) 362-4816
TXDOT	CURTIS RABENALDT, SIGNALS	(210) 207-7765
COSA TRAFFIC	BRAD ADAMI, TRANSGUIDE ADRIAN OLGUIN	(210) 731-5109 (210) 207-7769

- UTILITIES SHALL BE LOCATED BY CALLING 1-800-DIG-TEST. THE FAA AND THE CONTRACTOR SHALL UNCOVER ALL EXISTING UTILITIES AND VERIFY EXISTING ELEVATIONS OF SAME AT ALL UTILITY CROSSINGS BEFORE COMMENCING ANY OTHER WORK. CONFLICTS SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY.
- EACH CONSTRUCTION AREA SHALL BE SHAPED TO ALLOW DRAINAGE OF SURFACE OR GROUND WATER DURING EACH WORK OPERATION. IF NECESSARY, SURFACE OR GROUND WATER SHALL BE PUMPED IMMEDIATELY FROM EACH CONSTRUCTION AREA IN COMPLIANCE WITH THE EPA OR TNRC REGULATIONS. THE COST OF DEWATERING SHALL BE SUBSIDIARY TO THE CONSTRUCTION.
- CONTRACTOR SHALL MAKE ALL PROVISIONS INCLUDING UTILITIES, TO ESTABLISH HIS/HER STAGING AREA. THE CONTRACTOR SHALL PROVIDE SECURITY FENCE AROUND THE UTILIZED STAGING AREA. THIS SECURITY FENCE SHALL INCLUDE VISUAL SCREENING FROM PUBLIC AREAS. UPON COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL RESTORE THE STAGING AREA INCLUDING REMOVAL OF UTILITY SERVICE LINES, GRADING, TOPSOIL, AND SEEDING AS NECESSARY TO THE SATISFACTION OF THE CM TO RESTORE THE STAGING AREA TO ITS ORIGINAL CONDITION. NO SEPARATE PAY ITEM FOR WORK ASSOCIATED WITH ESTABLISHING AND RESTORING THE CONTRACTOR STAGING AREA IS INCLUDED.
- CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND ACQUIRING AN ACCEPTABLE WATER SOURCE AS NECESSARY FOR CONSTRUCTION.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL HORIZONTAL AND VERTICAL CONSTRUCTION STAKING AS REQUIRED FOR THE PROJECT DEVELOPMENT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING HIS/HER OWN PROJECT OFFICE AND COMPLETE UTILITY SERVICES. THE AIRPORT WILL NOT PROVIDE FACILITIES AND SERVICES TO THE CONTRACTOR DURING CONSTRUCTION. THE CONTRACTOR'S PERSONNEL ARE PROHIBITED FROM UTILIZING THE AIRPORT TERMINAL OR ANY TENANT FACILITIES.
- THE CONTRACTOR SHALL VIDEO TAPE THE ENTIRE WORK AREA AFTER THE PRE-CONSTRUCTION MEETING AND PRIOR TO MOBILIZATION OF PERSONNEL AND EQUIPMENT. IN AREAS WHERE CONSTRUCTION EQUIPMENT CROSSES EXISTING PAVEMENTS, THE CONTRACTOR SHALL TAKE PICTURES OR VIDEO OF THE PAVEMENT PRIOR TO COMMENCING OPERATIONS. THE CONTRACTOR SHALL PROVIDE ONE (1) COPY OF ALL VIDEO AND/OR PHOTOGRAPHS TO THE CONSTRUCTION MANAGER. THIS DOCUMENTATION SHALL BE USED TO DETERMINE THE AMOUNT OF DAMAGE, IF ANY, CAUSED TO THE PAVEMENTS AND SURROUNDING FACILITIES BY THE CONSTRUCTION EQUIPMENT AND THE QUALITY OF CONSTRUCTION WHICH SHALL BE REQUIRED FOR THE REPAIRS, IF ANY.

GENERAL NOTES (CONT'D):

- ALL SAWCUTTING SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS IN THE CONTRACT.
- BI-WEEKLY PROGRESS MEETINGS ARE MANDATORY. THE DAY OF THE WEEK FOR THESE MEETINGS WILL BE DETERMINED BY AGREEMENT BETWEEN THE AIRPORT MANAGEMENT, THE CONSTRUCTION MANAGER AND THE CONTRACTOR, AT A MINIMUM, THE MEETING WILL BE ATTENDED BY THE PROJECT SUPERINTENDENT, CONTRACTING SUPERINTENDENT, FOREMEN OF ACTIVE WORK (INCLUDING SUBS) AND THE CONTRACTOR'S QC REPRESENTATIVE.
- THE CONTRACTOR SHALL SUBMIT HIS/HER CONSTRUCTION WORK SCHEDULE TO THE CONSTRUCTION MANAGER EACH WEEK PROJECTING UPCOMING WORK FOR THE NEXT THREE WEEKS.
- THE CONTRACTOR'S PROJECT SUPERINTENDENT SHALL PARTICIPATE IN ALL COORDINATION MEETINGS AND SHALL BE ON SITE DURING ALL ACTIVITIES.
- HAUL TRUCKS TRANSPORTING LOOSE MATERIALS SHALL USE LOAD COVERS AND SHALL BE LOADED SUCH THAT NO SPILLAGE OCCURS DURING TRANSIT ON THE STATE, MUNICIPAL, OR AIRPORT ROADWAYS, TAXIWAYS OR APRONS. ALL CONTRACTOR EQUIPMENT MUST BE MAINTAINED GOOD WORKING ORDER. CONTRACTOR VEHICLES THAT LEAK OR DRIP OIL AND/OR FLUIDS ONTO ANY APRON, RAMP OR TAXIWAY MUST BE REMOVED FROM THE PROJECT FOR REPAIR.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE ALL OFF-AIRPORT ROUTES WITH THE APPROPRIATE AUTHORITIES. ON-AIRPORT HAULING SHALL BE COORDINATED WITH CM AND APPROVED IN ADVANCE TO MINIMIZE IMPACT TO AIRPORT OPERATIONS.
- CONSTRUCTION EQUIPMENT AND VEHICLES SHALL TRAVEL A MINIMUM AMOUNT ON NEWLY CONSTRUCTED PAVEMENTS SO THAT THESE AREAS WILL NOT BE DAMAGED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO THE NEWLY CONSTRUCTED PAVEMENTS AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE EFFORT AND COST OF IMMEDIATE CLEANING OF EARTH TRACKING AND SPILLS ON PAVED SURFACES RESULTING FROM THE CONTRACTOR'S OPERATIONS. BECAUSE OF THE POTENTIAL FOR EXTREME DAMAGE TO AIRCRAFT ENGINES BY INGESTION OF FOREIGN OBJECTS, THE CONTRACTOR SHALL OPERATE AND MAINTAIN MECHANICAL SWEEPER/VACUUM (WET/DRY) EQUIPMENT COMPLETE WITH OPERATORS ON THE PROJECT DURING ANY HAULING OR OTHER OPERATIONS ACROSS TAXIWAYS, TAXILANES OR APRONS. AT A MINIMUM, ONE VAC TRUCK PER CROSSING OF ACTIVE TAXIWAY OR RUNWAY IS REQUIRED.
- BATCH PLANTS ARE PERMITTED AT THE DESIGNATED LOCATION. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL APPLICABLE PERMITS. THE BATCH PLANT MUST BE LIT WITH AN OBSTRUCTION LIGHT MEETING FAA CRITERIA FOR OBSTRUCTION LIGHTING.
- CONTRACTOR SHALL PROVIDE AND INSTALL ALL MATERIALS AND DRAINAGE STRUCTURES TO CONSTRUCT A STABILIZED CONSTRUCTION ENTRANCE/EXIT AT EACH POINT SHOWN ON THE PLANS. THE COST OF ANY DRAINING STRUCTURES SHALL BE SUBSIDIARY TO ITEM 100-MOBILIZATION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE IMMEDIATE REPAIR OF ANY DAMAGE TO AIRPORT/AIRLINE FACILITIES, INCLUDING BUT NOT LIMITED TO EXISTING PAVEMENTS, UNDERGROUND CABLES, LIGHTS, SIGNS, BUILDINGS, FENCE, EQUIPMENT, ETC., CAUSED DURING CONSTRUCTION. ALL REPAIRS MUST BE MADE IN COORDINATION WITH THE CM AND TO EQUAL OR BETTER QUALITY AND IN COMPLIANCE WITH AIRPORT AND/OR FAA REQUIREMENTS. ALL REPAIR AND REPLACEMENT COSTS SHALL BE AT THE EXPENSE OF THE CONTRACTOR.
- ALL MATERIAL SUBMITTALS FOR ITEMS TO BE USED IN CONSTRUCTION OF THE PROJECT SHALL BE SUBMITTED TO THE CONSTRUCTION MANAGER FOR REVIEW AND APPROVAL A MINIMUM OF 10 DAYS PRIOR TO COMMENCEMENT OF THE ASSOCIATED WORK.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL AND DISPOSAL OF ALL WASTE MATERIAL GENERATED DURING CONSTRUCTION. WASTE MATERIAL MUST BE REMOVED FROM THE WORK SITE AND DISPOSED OF IN SUCH A MANNER AS TO NOT DAMAGE THE OWNER'S OR OTHER PERSON'S PROPERTY. (NO SEPARATE PAY FOR THE ITEM)
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING REQUIRED SECURITY TO PROTECT HIS OWN PROPERTY, EQUIPMENT, AND WORK IN PROGRESS.
- VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD BEFORE COMMENCING ANY WORK. IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO REPORT ANY DISCREPANCIES TO THE OWNER IN A TIMELY MANNER.
- THE DRAWINGS SHOW AS MUCH INFORMATION AS CAN BE REASONABLY OBTAINED FROM AN ON THE GROUND OBSERVATION, SURVEY AND EXISTING CONSTRUCTION DRAWINGS REGARDING THE TOPOGRAPHIC FEATURES, AND ELEVATIONS, AS WELL AS THE LOCATION AND NATURE OF PIPELINES, UNDERGROUND CABLES, UTILITIES, ETC., HOWEVER, THE ACCURACY OF OR COMPLETENESS OF SUCH INFORMATION IS NOT GUARANTEED.
- NO OPEN TRENCHES OR DROP OFFS ARE PERMITTED. AREA NEEDS TO BE FENCED OFF WITH REFLECTIVE SAFETY FENCE AND/OR STEEL PLATES MAY NEED TO BE USED.
- HORIZONTAL CONTROL MONUMENTS ARE SHOWN ON SHEET C1.12. CONTRACTOR IS RESPONSIBLE FOR PROVIDING OTHER LAYOUT AND GRADE CONTROL SURVEYING FOR CONSTRUCTION OF THE PROJECT.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PERMANENT SERVICE TERMINATION ASSOCIATED WITH UTILITY LINES TO BE REMOVED AND ABANDONED.
- CONTRACTOR SHALL MAINTAIN A COPY OF ALL NECESSARY PERMITS ON THE JOBSITE.
- ALL EXCAVATION IS UNCLASSIFIED AND SHALL INCLUDE ROCK AND ALL OTHER MATERIALS ENCOUNTERED REGARDLESS OF THEIR NATURE OR THE MANNER IN WHICH THEY ARE REMOVED.
- WHERE FINISHED CONTOURS ARE SHOWN TO MATCH EXISTING CONTOURS ON THE GRADING PLANS, NO GRADE SEPARATION IS ALLOWABLE. CONTRACTOR SHALL ADJUST GRADING AS NECESSARY TO ACCOMPLISH THIS REQUIREMENT.
- CONTRACTOR AND/OR CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR STRUCTURAL DESIGN/GEOTECHNICAL/SAFETY/EQUIPMENT CONSULTANT, IF ANY, SHALL REVIEW THESE PLANS AND AVAILABLE GEOTECHNICAL INFORMATION AND THE ANTICIPATED INSTALLATION SITE(S) WITHIN THE PROJECT WORK AREA IN ORDER TO IMPLEMENT CONTRACTOR'S TRENCH EXCAVATION SAFETY PROTECTION SYSTEMS, PROGRAMS AND/OR PROCEDURES FOR THE PROJECT DESCRIBED IN THE CONTRACTOR DOCUMENTS. THE CONTRACTOR'S IMPLEMENTATION OF THESE SYSTEMS, PROGRAMS AND/OR PROCEDURES SHALL PROVIDE ADEQUATE TRENCH EXCAVATION SAFETY PROTECTION THAT COMPLY WITH AS A MINIMUM, OSHA STANDARDS FOR TRENCH EXCAVATIONS. SPECIFICALLY CONTRACTOR AND/OR CONTRACTOR'S INDEPENDENTLY RETAINED EMPLOYEE OR SAFETY CONSULTANT SHALL IMPLEMENT A TRENCH SAFETY PROGRAM IN ACCORDANCE WITH OSHA STANDARDS COVERING THE PRESENCE AND ACTIVITY OF INDIVIDUALS WORKING IN AND AROUND TRENCH EXCAVATION.
- CONTRACTOR SHALL FURNISH THE ENGINEER WITH AN AS-BUILT PLAN INDICATING THE ACTUAL MEASUREMENT AND LOCATION OF UTILITY LINES AND SITE IMPROVEMENTS INSTALLED OR ENCOUNTERED.
- CONTRACTOR SHALL MAINTAIN SERVICE TO EXISTING SANITARY SEWERS AT ALL TIMES DURING CONSTRUCTION.
- DUE TO FEDERAL REGULATIONS TITLE 49, PART 192.181, CITY PUBLIC SERVICE MUST MAINTAIN ACCESS TO GAS VALVES AT ALL TIMES. THE CONTRACTOR MUST PROTECT AND WORK AROUND GAS VALVES THAT ARE IN THE PROJECT AREAS.
- NO EXTRA PAYMENT WILL BE MADE FOR INCIDENTAL WORK OR MATERIALS REQUIRED TO COMPLETE A CONTRACT PAY ITEM. THIS INCIDENTAL WORK WILL BE REQUIRED AND SHALL BE INCLUDED UNDER THE PAY ITEM TO WHICH IT RELATES.
- WHENEVER POWER POLES ARE ADJACENT TO THE PROPOSED CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE PROPER SHORING, BRACING AND/OR OTHER SUITABLE SUPPORT FOR THE POLES DURING CONSTRUCTION (NO SEPARATE PAY ITEM)

GENERAL NOTES (CONT'D):

- OVERHEAD POWER LINES EXIST IN THE AREA OF THE PROJECT OR HAUL ROUTES. TEXAS LAW ARTICLE 1436C, PROHIBITS ALL ACTIVITIES IN WHICH PERSONS OR EQUIPMENT MAY COME WITHIN SIX (6) FEET OF ENERGIZED OVERHEAD POWER LINES, AND FEDERAL REGULATIONS, TITLE 29, PART 1910.180(I) AND PART (1926.550(A) (15) REQUIRE A MINIMUM OF TEN (10) FEET FROM THESE FACILITIES WHERE CONTRACTOR MUST WORK NEAR OVERHEAD POWER LINES. CALL (210) 353-2012 FOR THE LINES TO BE DE-ENERGIZED AND/OR MOVED AT CONTRACTOR'S EXPENSE. NEED TO PROVIDE FAA TECH OPS (7) BUSINESS DAYS PRIOR NOTICE; AIRFIELD FACILITIES MAINTENANCE (3) BUSINESS DAYS PRIOR NOTICE; CPS ENERGY (1.5) MONTHS PRIOR NOTICE.
- BLASTING IS NOT ALLOWED.
- WATER LINE CROSSING: WHERE THE MINIMUM 9 FOOT SEPARATION DISTANCE BETWEEN SEWER LINES AND WATER LINES/MAINS CANNOT BE MAINTAINED, INSTALLATION OF WATER AND SEWER LINE SHALL BE IN STRICT ACCORDANCE WITH THE TEXAS COMMISSION ON ENVIRONMENTAL QUALITY (30 TAC 290.44(E) AND 317.13 APPENDIX E)
- EROSION AND SEDIMENTATION CONTROL: STORMWATER POLLUTION PREVENTION PLANS AND PROCEDURES SHALL BE IMPLEMENTED ACCORDING TO TCEQ AND EPA REGULATIONS FOR STORMWATER DISCHARGE FROM CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL MEET ALL REQUIREMENTS OF THE ATTACHED SW3P PLAN AND THESE REGULATIONS. FINAL PROJECT ACCEPTANCE SHALL NOT BE GRANTED UNTIL ALL PERMANENT STABILIZATION MEASURES HAVE BEEN ESTABLISHED. FURTHER INFORMATION REGARDING SAN ANTONIO AIRPORT SYSTEM'S SW3P, SOIL MANAGEMENT PLAN, FUEL SPILL RESPONSE PLAN, AND NOISE MANAGEMENT PROGRAM CAN BE FOUND AT AVIATION DEPARTMENT'S WEBSITE UNDER ENVIRONMENTAL STEWARDSHIP: HTTP://WWW.SANANTONIO.GOV/AVIATION/
- SANITARY FACILITIES ARE TO BE SUPPLIED BY THE CONTRACTOR AND CAN ONLY BE LOCATED WITHIN SECURED CONSTRUCTION AREAS.
- ALL ACCESS ROADS TO STORAGE AREAS AND/OR WORK AREAS ARE TO BE DEVELOPED AND MAINTAINED BY THE CONTRACTORS. THESE ROADS ARE TO BE RETURNED TO THEIR ORIGINAL OR BETTER CONDITION BY THE CONTRACTOR UPON COMPLETION OF THE PROJECT.
- THE CONTRACTOR'S JOB SUPERINTENDENT IS TO MEET WITH THE RESIDENT PROJECT INSPECTOR PRIOR TO THE START OF EACH WORKING DAY TO COORDINATE DAILY CONSTRUCTION.
- THE CONTRACTOR IS TO COORDINATE WITH THE RESIDENT PROJECT INSPECTOR AND THE AVIATION DEPARTMENT CONCERNING SAFETY IN ASSURING THAT POWER IS OFF IN THE SYSTEM WHILE THE ELECTRICAL WORK IS UNDERWAY. SPECIFICALLY, THEY ARE TO AGREE ON A CONTROL AND CHECK PROCEDURE TO PREVENT ACCIDENTAL POWER RECONNECT. THESE AGREED UPON PROCEDURES ARE TO BE DOCUMENTED IN A WRITTEN OUTLINE, SIGNED BY BOTH GROUPS AND A COPY ON FILE WITH THE RESIDENT PROJECT INSPECTOR AND THE AVIATION DEPARTMENT BEFORE ANY SUCH WORK IS UNDERTAKEN.
- CONTRACTOR SHALL VERIFY THE SIZE OF EXISTING CABLES WHERE SPLICES AND CONNECTORS OCCUR IN ORDER TO PROVIDE THE PROPER SPLICE CONNECTOR KITS.
- AT NO TIME SHALL ANY EXCAVATION AREAS BE LEFT UNATTENDED UNLESS APPROPRIATE MARKING AND BARRICADING IS EMPLOYED.
- DEMOLITION AND EXCAVATION SOILS:
 - BUILDING MATERIALS, UNCLASSIFIED DEMOLITION SOILS AND FENCING SHALL BE LEGALLY DISPOSED OF OUTSIDE AIRPORT PROPERTY.
 - CONTAMINATED SOILS, IF ENCOUNTERED, SHALL BE STOCKPILED AND COVERED WITH 10 MIL VISQUINE TO PREVENT RAINWATER CONTAMINATION AND UNCONTROLLED VAPOR EMISSIONS. SEE OVERALL PHASING PLAN FOR DUMP LOCATION PRIOR TO REMOVAL OFF SITE.
 - CONCRETE RUBBLE, CEMENT STABILIZED BASE, REINFORCING STEEL AND ASPHALT SHALL BE LEGALLY DISPOSED OF OUTSIDE AIRPORT PROPERTY.
 - UNCONTAMINATED SOILS SHALL BE STOCKPILED WITHIN THE PHASING LIMITS INCLUDING HEIGHT RESTRICTIONS UNTIL SUCH TIME THAT THEY ARE RE-USED OR REMOVED FROM THE AIRPORT.
- SEE STORM WATER POLLUTION PREVENTION PLAN FOR TURF REQUIREMENTS.
- ALL CONCRETE FOR STRUCTURES, CURBS, RIPRAP, ETC., SHALL CONFORM TO FAA SPECIFICATION P-610.
- ALL APRON AND TAXIWAY CONCRETE SHALL CONFORM TO FAA SPECIFICATION P-601.
- CONTRACTOR IS RESPONSIBLE FOR MAINTENANCE OF ALL UTILITIES IN THE PROJECT AREA UNDER DEMOLITION AND CONSTRUCTION.
- RESTRICTED AIRPORT AREA MUST REMAIN SECURE AT ALL TIMES. TEMPORARY FENCE ADEQUATE FOR THIS PURPOSE SHALL BE ERRECTED PRIOR TO DEMOLITION AND ERECTION OF PERMANENT SECURITY FENCE. NO SEPARATE PAY ITEM FOR TEMPORARY FENCES.
- EMPLOYEE PARKING WILL NOT BE PROVIDED BY OWNER. CONTRACTOR SHALL ARRANGE FOR OFF-SITE PARKING AND TRANSPORTATION TO JOB SITE.
- ALL PERSONNEL OPERATING VEHICLES IN THE AOA MUST HAVE RECEIVED TRAINING AND HAVE AN AIRPORT DRIVING LICENSE.

SPECIAL CONDITIONS FOR CONSTRUCTION WITHIN THE AIR OPERATIONS AREA:

SAN ANTONIO INTERNATIONAL AIRPORT MAINTAINS OPERATIONS 24 HOURS A DAY, 365 DAYS PER YEAR FOR GENERAL AVIATION AND AIR CARRIER SERVICE. SAFETY, SECURITY AND OPERATIONS WILL TAKE PRECEDENCE OVER ALL CONSTRUCTION ACTIVITIES WITH THE AIRPORT SECURITY. AS SUCH, ALL CONSTRUCTION ACTIVITY SHALL BE CONDUCTED IN ACCORDANCE WITH THE STIPULATIONS OUTLINED BELOW:

SECURITY:
FINES: ANY FINES ASSESSED TO THE AVIATION DEPARTMENT DUE TO THE CONTRACTOR'S VIOLATIONS OF ANY SECURITY REQUIREMENTS WILL BE PASSED ON TO THE CONTRACTOR.

BADGING: PERSONNEL WORKING WITHIN THE SIDA/AOA MUST DISPLAY CURRENT AIRPORT ISSUED IDENTIFICATION AT ALL TIMES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING THE NECESSARY BACKGROUND CHECKS FOR THESE PERSONNEL AS WELL AS MAINTAINING PERSONNEL FILES FOR THE PROJECT DURATION. THESE FILES ARE SUBJECT TO FAA REVIEW. SUBSEQUENT TO THE PRECONSTRUCTION MEETING, THE CONTRACTOR MUST ARRANGE FOR ALL BADGED PERSONNEL TO ATTEND THE SIDA CLASS. NOT ALL PERSONNEL MUST BE BADGED. IT IS PERMISSIBLE TO HAVE ONE BADGED PERSONNEL ESCORT AND BE RESPONSIBLE FOR ALL UNBADGED PERSONS AT THE DISCRETION OF AVIATION SECURITY. ALL UNSCORTED EQUIPMENT OPERATORS AND DRIVERS MUST BE BADGED. SIDA CLASSES ARE COORDINATED THROUGH AIRPORT SECURITY AND TAKE APPROXIMATELY TWO HOURS. ALL BADGES REMAIN THE PROPERTY OF THE AVIATION DEPARTMENT AND MUST BE RETURNED AT THE COMPLETION OF THE CONTRACT.

CONSTRUCTION ACCESS: THE CONTRACTOR WILL BE ISSUED INGRESS/EGRESS INTO THE SIDA/AOA AT A POINT DESIGNATED BY THE AVIATION DEPARTMENT. THIS POINT WILL BE AS CLOSE AS PRACTICABLE TO THE MARSHALING AREA AS POSSIBLE. THE CONTRACTOR WILL BE REQUIRED TO ENTER INTO A SECURITY AMENDMENT WHICH SHALL BE ISSUED AND KEPT ON FILE WITH AIRPORT POLICE. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN SECURITY AT ALL TIMES DURING THE DURATION OF THE CONTRACT. THE ACCESS WILL BE LOCKED AT ALL TIMES WHILE UNATTENDED AND SHALL BE MANNED BY APPROVED BADGED PERSONNEL ONLY. ANY CONTRACTOR ASSIGNED TO THE PROJECT WHO HAS A BUSINESS RELATED NEED TO BE PRESENT AT THE SECURED AREA OR STERILE AREA FOR MORE THAN 14 DAYS (CONSECUTIVE OR INTERMITTENTLY) MUST BE PROCESSED FOR A SIDA BADGE. THIS POLICY IS APPLIED TO ALL WORKERS, INCLUDING SUBCONTRACTOR AND SUPPLIERS AND ETC. ESCORT RATIO OF 2:1 APPLIES ONLY FOR WORK THAT IS LESS THEN 14 DAYS.

SPECIAL CONDITIONS FOR CONSTRUCTION WITHIN THE AIR OPERATIONS AREA (CONT'D):

FENCING:
RESTRICTED AREA SHALL BE KEPT SECURE AT ALL TIMES BY TEMPORARY AND/OR PERMANENT SECURITY FENCES.

INSURANCE:
IN ADDITION TO THE INSURANCE REQUIREMENTS OUTLINED IN THE GENERAL CONDITIONS, THE CONTRACTOR SHALL PROVIDE ADDITIONAL EXCESS (UMBRELLA) LIABILITY INSURANCE OF NOT LESS THAN \$15,000,000 (FIFTEEN MILLION DOLLARS), THE COST OF WHICH SHALL BE INCLUDED IN ITEM C0-11.1.2 "ADDITIONAL INSURANCE COVERAGE".

OPERATIONS:
AIRPORT OPERATIONS IS CHARGED WITH THE RESPONSIBILITY OF OVERSEEING THE DAILY OPERATIONS OF ALL ACTIVITIES WITHIN THE AIRPORT, INCLUDING THE PROPER EXECUTION OF ALL FACETS OF THE CONSTRUCTION ACTIVITIES AND COMPLIANCE WITH APPLICABLE FEDERAL REGULATIONS. THE CONTRACTOR SHALL COMPLY WITH ALL DIRECTIONS ISSUED BY OPERATIONS IN A TIMELY MANNER.

VEHICULAR OPERATIONS: IN ADDITION TO BEING BADGED, ALL UNSCORTED EQUIPMENT OPERATORS AND DRIVERS MUST ATTEND THE REQUIRED DRIVING COURSE SPONSORED BY AIRPORT OPERATIONS. THIS COURSE TAKES APPROXIMATELY 4 HOURS. NO VEHICLES MAY BE OPERATED WITHIN THE AIR OPERATION AREA (AOA) UNTIL THE DRIVERS SUCCESSFULLY PASS THE DRIVING COURSE AND SATISFACTORY INSURANCE CERTIFICATES ARE ON FILE WITH THE OFFICE OF PLANNING & ENGINEERING (REFER TO SECTION ENTITLED "INSURANCE" ABOVE). ALL VEHICULAR MOVEMENT AREAS WILL BE SUBJECT TO THE APPROVAL OF THE AVIATION DEPARTMENT AND THE VEHICLES OPERATING ON AIRPORT PROPERTY UTILIZE AN FAA APPROVED FLAG ATTACHED TO THE VEHICLE TOGETHER WITH 12" COMPANY LOGOS OR 6" LETTERS IDENTIFYING THE COMPANY, DISPLAYED ON BOTH DOORS.

ESCORT: ALL VEHICLES AND EQUIPMENT BEING OPERATED BY PERSONNEL WITHOUT HAVING SUCCESSFULLY COMPLETED THE DRIVING COURSE MUST BE ESCORTED BY VEHICLES OPERATED BY PERSONNEL AUTHORIZED TO DRIVE WITHIN THE SIDA/AOA. THE ESCORT MUST BE PROXIMATE TO THE VEHICLES AT ALL TIMES WHILE WITHIN THE SIDA/AOA INCLUDING DEPARTING THROUGH THE CONSTRUCTION ACCESS. SHOULD THE NEED ARISE TO ENTER OR CROSS AN ACTIVE RUNWAY, THE ESCORT MUST BE DONE BY PERSONNEL FROM AIRPORT OPERATIONS AND COORDINATED THROUGH AIRPORT OPERATIONS.

STORAGE & MARSHALLING AREAS
EXCESS MATERIAL: ALL MATERIAL REMOVED FROM THE CONSTRUCTION SITE WHICH IS NOT TO BE USED IN THE FINAL CONSTRUCTION SHALL BE REMOVED FROM THE AIRPORT PROPERTY AND DISPOSED OF IN A LEGAL MANNER. NO EXCESS MATERIAL SHALL BE DISPOSED OF ON AIRPORT PROPERTY UNLESS PRIOR WRITTEN APPROVAL FROM THE AVIATION DEPARTMENT IS ISSUED. SUITABLE MATERIAL MAY BE HAULED AND DISPOSED AT AIRPORT'S STOCKPILE SITE, AND SHALL BE FREE FROM ALL OF THE FOLLOWING:

- CONCRETE GREATER THAN 3" IN ANY DIMENSION
- ASPHALT OR BITUMINOUS MATERIAL
- PIPE OR CONDUIT
- CABLES AND WIRES
- CEMENT TREATED MATERIAL GREATER THAN 3" IN ANY DIMENSION
- METAL PRODUCTS OF ANY KIND
- TRASH, GARBAGE OR ORGANIC MATERIAL
- CONTAMINATED MATERIALS

CONSTRUCTION MATERIALS: CONSTRUCTION MATERIALS NOT FOR IMMEDIATE USE SHALL BE STOCKPILED AT THE DESIGNATED MARSHALING AREA. OTHER MATERIALS MAY BE STORED AT APPROVED LOCATIONS APPROXIMATE TO THE WORK AREA PROVIDED HOWEVER, THAT THE PILES ARE NO GREATER THAN 18" IN HEIGHT. HIGHER PILES MAY BE PERMITTED ONLY DURING WORKING HOURS AND IN SUCH QUANTITY THAT THEY MAY BE REDUCED IN HEIGHT TO 18" MAXIMUM WITHIN 30 MINUTES OF NOTIFICATION.

EQUIPMENT: ALL EQUIPMENT SHALL BE STORED AT THE DESIGNATED STORAGE AREAS DURING NON-WORKING TIMES. SPECIAL PROVISIONS MUST BE GRANTED FOR EQUIPMENT WHICH IS NOT READILY MOVABLE TO BE STORED ELSEWHERE UPON APPROVAL BY THE AVIATION DEPARTMENT.

NAVIGATIONAL EQUIPMENT:
TESTING: THE FAA MAINTAINS VARIOUS FORMS OF NAVIGATIONAL EQUIPMENT AND APPURTENANCES THROUGHOUT THE AIRPORT. THERE MAY BE TIMES DURING THE CONSTRUCTION WHERE TESTS AND/OR EQUIPMENT CHECKS MUST BE RUN TO MAINTAIN SERVICEABILITY. THE CONTRACTOR SHALL COMPLY WITH ALL REQUESTS AND DIRECTIVES DURING THE PROSECUTION OF THE TESTS. TIMELY NOTIFICATION OF SUCH TESTS CANNOT BE GUARANTEED.

CABLES: COMMUNICATION AND NAVIGATIONAL CABLES TRAVERSE THE CONSTRUCTION AREA. EVERY EFFORT HAS BEEN MADE TO IDENTIFY AND PROPERLY DEPICT THE LOCATIONS OF ALL CABLES ON THE DRAWINGS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE AVIATION DEPARTMENT OF (210) 207-3519, 48 HOURS IN ADVANCE OF ANY CONSTRUCTION IN ORDER THAT THE CABLES WILL BE LOCATED OR CUT OR OTHERWISE DAMAGED. HE SHALL IDENTIFIED. THE CONTRACTOR SHALL IDENTIFY THE LOCATION AND ALIGNMENT OF THESE CABLES ALONG AND THROUGHOUT THE ENTIRE PROJECT AREA USING ORANGE SAFETY NETTING. GAPS TO FACILITATE VEHICULAR CROSSINGS WILL BE PERMITTED AS REQUIRED. THESE CROSSINGS SHALL BE PROTECTED USING 1/2" THICK BY 8' LONG STEEL PLATES PLACED ALONG THE FULL WIDTH OF EACH CROSSING. NO CONSTRUCTION SHALL COMMENCE PRIOR TO THE LOCATION OF THE CABLES. SHOULD THE CONTRACTOR ENCOUNTER CABLES NOT IDENTIFIED ON THE DRAWINGS OR FIELD LOCATED OR CUT OR OTHERWISE DAMAGED, HE SHALL IMMEDIATELY CEASE OPERATIONS IN THE AREA AND NOTIFY THE AVIATION DEPARTMENT AT 207-3519 DURING THE DAY OR (210) 207-3519 AFTER HOURS, IN ORDER THAT THE FAA CAN IDENTIFY THE CABLE. UNLESS DIRECTED OTHERWISE, CUT OR DAMAGED CABLES SHALL BE REPLACED BETWEEN THE EXISTING PULL BOXES (TYPICALLY 2000 FEET APART). ALL SPLICING SHALL BE PERFORMED BY AN FAA CERTIFIED SPLICER. AFTER ALL SPLICES ARE COMPLETED, THE CABLES SHALL BE TESTED FOR CONTINUITY AND MEGERED BY THE CONTRACTOR.

LIGHTING SYSTEMS: THE AVIATION DEPARTMENT AND THE FAA MAINTAINS VARIOUS FORMS OF LIGHTING SYSTEMS THROUGHOUT THE AIRPORT. ALL PORTIONS OF THESE SYSTEMS SHALL BE PROPERLY LOCATED AND IDENTIFIED PRIOR TO THE START OF CONSTRUCTION. CONTRACTOR TO CONTACT THE AVIATION DEPARTMENT AT (210) 207-3519, 48 HOURS IN ADVANCE OF ANY CONSTRUCTION IN ORDER THAT THESE SYSTEMS MAY BE PROPERLY IDENTIFIED. ALL ELECTRICAL WORK SHALL BE COORDINATED THROUGH THE AVIATION DEPARTMENT TO INSURE CIRCUIT OUTAGES ARE AVOIDED. EXCAVATED AREAS SHALL NOT BE LEFT UNATTENDED UNLESS APPROPRIATE BARRICADES ARE PROVIDED. THE CONTRACTOR SHALL HAVE A DESIGNATED CONTACT ON FILE WITH AIRPORT OPERATIONS WHO SHALL BE ON CALL 24 HOURS A DAY IN ORDER TO MAINTAIN THE BARRICADES. LOCATION AND PLACEMENT SHALL BE ADJUSTED AS NECESSITATED BY CHANGES IN CONSTRUCTION PROGRESS.

ALL LABOR, MATERIALS, TOOLS, CABLES AND CONNECTORS NECESSARY TO PROVIDE TEMPORARY CIRCUITS AS REQUIRED SHALL BE PROVIDED BY THE CONTRACTOR. FAA REQUIRES 7 BUSINESS DAYS PRIOR NOTICE.

BARRICADES:
ALL CONSTRUCTION AREAS SHALL BE PROPERLY BARRICADED, SIGNED AND MARKED AS DIRECTED BY THE AVIATION DEPARTMENT IN ACCORDANCE WITH THE BARRICADES SHOWN ON THE DRAWINGS. BARRICADES SHALL BE PROPERLY SECURED AS NECESSARY TO PREVENT OVERTURNING OR DISPLACEMENT FROM WIND OR JET BLAST AND SHALL BE ILLUMINATED. REFER TO FAA AC 150/5340-1G "STANDARD FOR AIRPORT MARKINGS" FOR FAA LIGHTING AND MARKING STANDARDS. EXCAVATED AREAS SHALL NOT BE LEFT UNATTENDED UNLESS APPROPRIATE BARRICADES ARE PROVIDED. THE CONTRACTOR SHALL HAVE A DESIGNATED CONTACT ON FILE WITH AIRPORT OPERATIONS WHO SHALL BE ON CALL 24 HOURS A DAY IN ORDER TO MAINTAIN THE BARRICADES. LOCATION AND PLACEMENT SHALL BE ADJUSTED AS NECESSITATED BY CHANGES IN CONSTRUCTION PROGRESS.

SAFETY NETTING: SHALL BE INSTALLED IN LOCATIONS AS SHOWN ON THE DRAWINGS OR AS DIRECTED BY THE RESIDENT PROJECT INSPECTOR. NETTING SHALL BE INSTALLED PRIOR TO THE START OF CONSTRUCTION AND SHALL BE MAINTAINED FOR THE DURATION OF THE PROJECT. ADJUSTMENTS IN LOCATION SHALL BE MADE AS DIRECTED BY THE AVIATION DEPARTMENT. REFER TO FAA AC 150/5340-1, CURRENT VERSION.



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NO. 928

RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)



07/22/13

MARK	DATE	DESCRIPTION

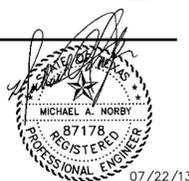
ISSUE:	-
PROJECT NO:	33-00178 and 33-00193
FILE NAME:	_33-00193-R1GN-003
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GENERAL NOTES



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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



MARK	DATE	DESCRIPTION

PROJECT NO:	33-00178 and 33-00193
FILE NAME:	33-00193-R1CO-401
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**CONSTRUCTION
PHASING PLAN
BASE BID**

LEGEND:

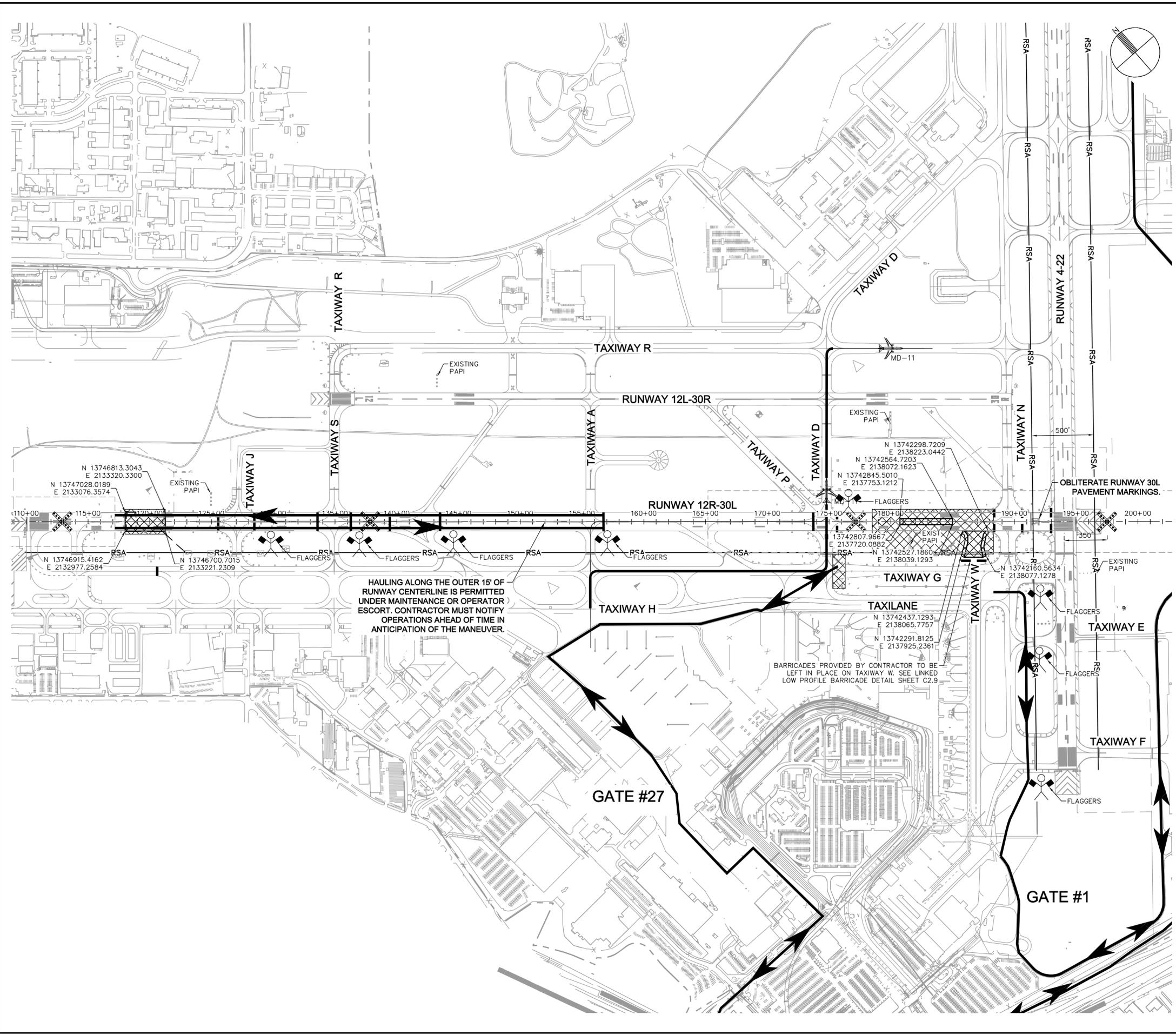
- BASE BID
- SAFETY FENCE
- LOW PROFILE BARRICADES
- HAUL ROUTE
- AIRCRAFT ROUTE
- PORTABLE LIGHTED RUNWAY CLOSURE MARKER
- FLAGGER (TO BE SUPPLIED BY CONTRACTOR)
- WING WALKER (TO BE SUPPLIED BY AIRLINES)

GENERAL PHASE NOTES

1. A SEPARATE NTP WILL BE ISSUED FOR EACH PHASE.
2. ACCESS TO THE WORK AREA IS SHOWN ON THE OVERALL PLAN SHEETS.
3. EXISTING AIRFIELD LIGHTING CIRCUITS MUST REMAIN IN SERVICE FOR ALL AREAS OPEN TO AIRCRAFT DURING ALL HOURS OF DARKNESS AND DURING INSTRUMENT METEOROLOGICAL CONDITIONS (BELOW 1000' CEILING OR LESS THAN 3 MILES OF VISIBILITY) UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE AIRPORT. NO DIRECT PAY FOR JUMPERS. CURRENT WEATHER CONDITIONS FOR THE AIRPORT MAY BE OBTAINED BY CALLING (210) 805-5583.
4. THE CONTRACTOR SHALL REMOVE OR COVER ALL AIRFIELD GUIDANCE SIGNS FOR RUNWAYS AND TAXIWAYS THAT ARE CLOSED DURING EACH PHASE (NO DIRECT PAY).
5. THE CONTRACTOR SHALL REMOVE ALL TAXIWAY CENTERLINES LEADING INTO THE CONSTRUCTION AREA PRIOR TO THE COMMENCEMENT OF EACH PHASE (PAVEMENT MARKING REMOVAL) (NPI).

BASE BID NOTES

1. BASE BID CONSISTS OF ALL WORK ON RUNWAY 12R-30L BETWEEN STATION 118+00 AND 121+25 AND BETWEEN STATION 180+74 AND 180+99. NORTHING/EASTING COORDINATES ARE TO PROVIDE THE GENERAL LOCATION OF EACH PHASE. REFER TO HORIZONTAL CONTROL AND DEMO PLANS FOR PHASING LIMITS.
2. FENCING AND BARRICADES MUST BE PLACED PER EACH PHASING SHEET FROM THE CENTERLINE OF RUNWAY AND TAXIWAYS.
3. RUNWAY 12R-30L WILL BE CLOSED TO AIRCRAFT OPERATIONS WHILE UNDER CONSTRUCTION.
4. RUNWAY 30L DESIGNATION MARKINGS SHALL BE OBLITERATED PRIOR TO BEGINNING THIS PHASE. OBLITERATION MUST TAKE PLACE AT NIGHT. DELAYS DUE TO OPERATIONS SHOULD BE EXPECTED.
5. LIGHTED RUNWAY CLOSURE "X'S" MUST BE PLACED ON THE RUNWAY NUMBERS ON THE WEST END (12R) AND 350' OFF THE EAST END (30L) OF THE RUNWAY AND TWO ADDITIONAL RUNWAY CLOSURE "X'S" MUST BE PLACED AS DIRECTED BY AIRPORT OPERATIONS (4 LIGHTED RUNWAY CLOSURE "X'S" TOTAL). "X'S" MUST BE MAINTAINED AND FUELED BY THE CONTRACTOR FOR THE DURATION OF THE PHASE WHILE THE RUNWAY IS CLOSED. LIGHTED RUNWAY CLOSURE "X'S" WILL REMAIN THE PROPERTY OF THE CONTRACTOR AT COMPLETION OF THE WORK.
6. BASE BID MAY NOT BE CONCURRENT WITH ANY OTHER PHASE PENDING NTP FROM THE AIRPORT. NTP WILL BE ISSUED FOR BASE BID PENDING FUNDING, ANTICIPATED WEATHER AND AIRPORT OPERATIONS.
7. BARRICADES MUST BE PLACED ACROSS THE RUNWAY ON BOTH SIDES OF EACH TAXIWAY CROSSING AT THE TAXIWAY OBJECT FREE AREA.

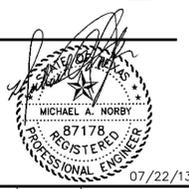


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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



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PROJECT NO:	33-00178 and 33-00193
FILE NAME:	_33-00193-R1CP-401
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**CONSTRUCTION
PHASING PLAN
ALT 1
PHASE 1A**

LEGEND:

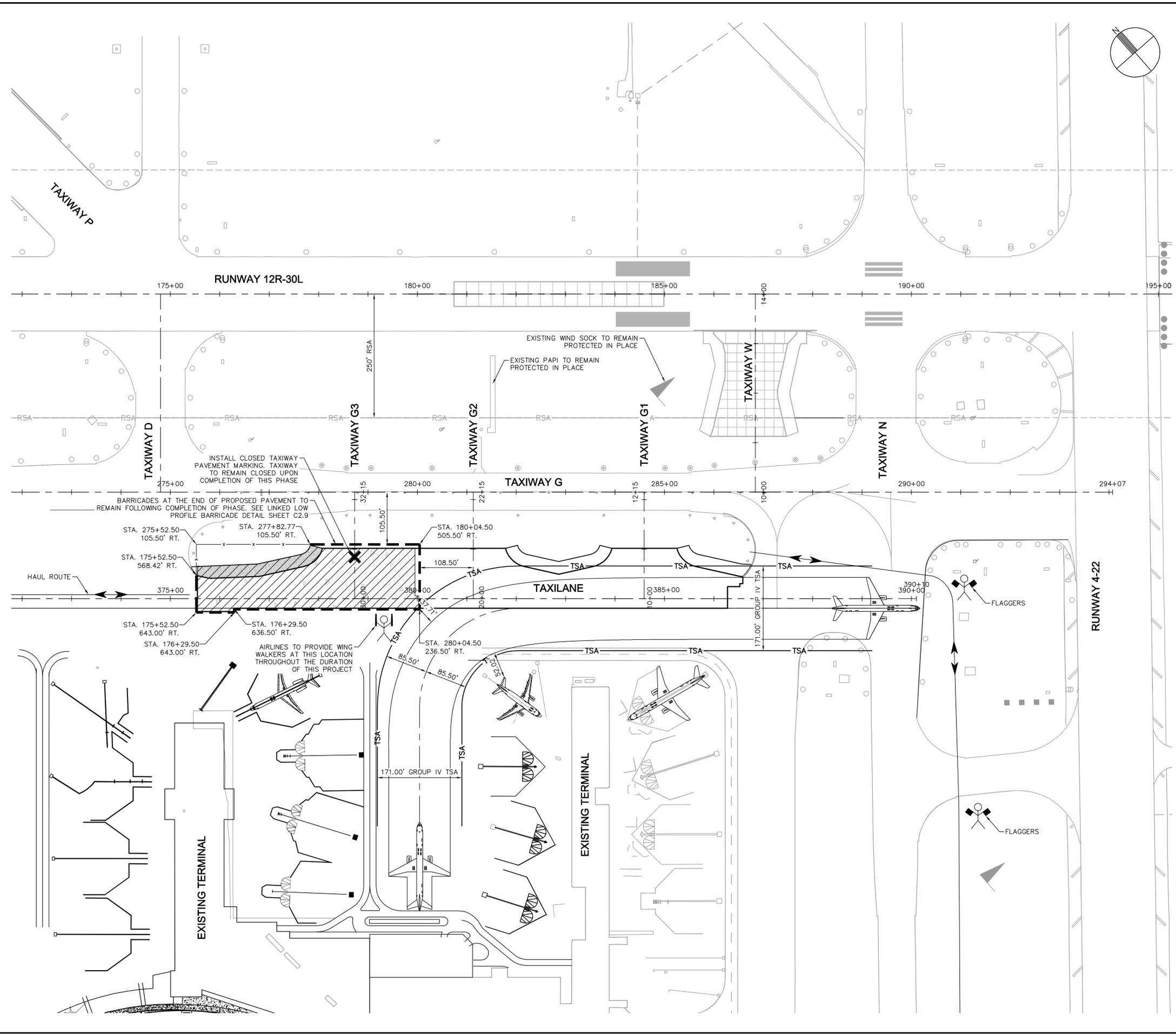
- PHASE 1A
- PHASE 1B
- PORTLAND CEMENT CONCRETE PAVEMENT
- ASPHALT CONCRETE SHOULDER PAVEMENT
- SAFETY FENCE
- LOW PROFILE BARRICADES
- HAUL ROUTE
- FLAGGER (TO BE SUPPLIED BY CONTRACTOR)
- WING WALKER (TO BE SUPPLIED BY AIRLINES)
- CLOSED RUNWAY/TAXIWAY PAVEMENT MARKING

GENERAL PHASE NOTES

- A SEPARATE NTP WILL BE ISSUED FOR EACH PHASE.
- ACCESS TO THE WORK AREA IS SHOWN ON THE OVERALL PLAN SHEETS.
- EXISTING AIRFIELD LIGHTING CIRCUITS MUST REMAIN IN SERVICE FOR ALL AREAS OPEN TO AIRCRAFT DURING ALL HOURS OF DARKNESS AND DURING INSTRUMENT METEOROLOGICAL CONDITIONS (BELOW 1000' CEILING OR LESS THAN 3 MILES OF VISIBILITY) UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE AIRPORT. NO DIRECT PAY FOR JUMPERS. CURRENT WEATHER CONDITIONS FOR THE AIRPORT MAY BE OBTAINED BY CALLING (210) 805-5583.
- THE CONTRACTOR SHALL REMOVE OR COVER ALL AIRFIELD GUIDANCE SIGNS FOR RUNWAYS AND TAXIWAYS THAT ARE CLOSED DURING EACH PHASE (NO DIRECT PAY).
- THE CONTRACTOR SHALL REMOVE ALL TAXIWAY CENTERLINES LEADING INTO THE CONSTRUCTION AREA PRIOR TO THE COMMENCEMENT OF EACH PHASE (PAVEMENT MARKING REMOVAL (NPI)).

ALTERNATE 1 PHASE 1A NOTES

- PHASE 1A CONSISTS OF THE NORTHWEST PORTION OF WORK ON THE TAXILANE. STATION/OFFSET COORDINATES ARE TO PROVIDE THE GENERAL LOCATION OF EACH PHASE. REFER TO GEOMETRIC AND DEMO PLANS FOR CONSTRUCTION LIMITS.
- FENCING AND BARRICADES MUST BE PLACED 250 FEET (OR MORE) FROM THE CENTERLINE OF RUNWAY 12R/30L, 85.5 FEET (OR MORE) FROM THE CENTERLINE OF THE INNER TAXILANE, 85.5 FEET (OR MORE) FROM THE CENTERLINE OF TAXIWAY G, AND 85.5 FEET (OR MORE) FROM THE CENTERLINE OF TAXIWAY N.
- THIS PORTION OF THE INNER TAXILANE BETWEEN TAXIWAY D AND THE TERMINAL APRON WILL BE CLOSED TO AIRCRAFT OPERATIONS UNDER THIS PHASE OF CONSTRUCTION.
- TAXIWAY N, TAXIWAY G, TAXIWAY D, AND THE SOUTHEAST PORTION OF THE TAXILANE WILL REMAIN OPEN TO GROUP IV AND LOWER AIRCRAFT. WING WALKERS WILL BE REQUIRED AT LOCATIONS SHOWN ON THE PLANS.
- PHASE 1A CANNOT BEGIN UNTIL THE BASE BID IS COMPLETE AND ACCEPTED.
- ANY WORK REQUIRED TO BE COMPLETED WITHIN A TAXILANE/TAXWAY SAFETY AREA WILL REQUIRE A TEMPORARY CLOSURE OF THE TAXILANE/TAXIWAY UNTIL THE WORK WITH THE SAFETY AREA IS COMPLETE.
- THE CONTRACTOR SHALL PROTECT IN PLACE THE PAPI AND THE WIND SOCK. ANY DAMAGE TO THE EQUIPMENT SHALL BE REPORTED IMMEDIATELY TO THE AIRPORT. THE CONTRACTOR MUST ALLOW ACCESS TO THESE FACILITIES BY THE FAA OR AIRPORT PERSONNEL FOR MAINTENANCE.



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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



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PROJECT NO: 33-00178 and 33-00193
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**CONSTRUCTION
PHASING PLAN
ALT 4
PHASE 4**

LEGEND:

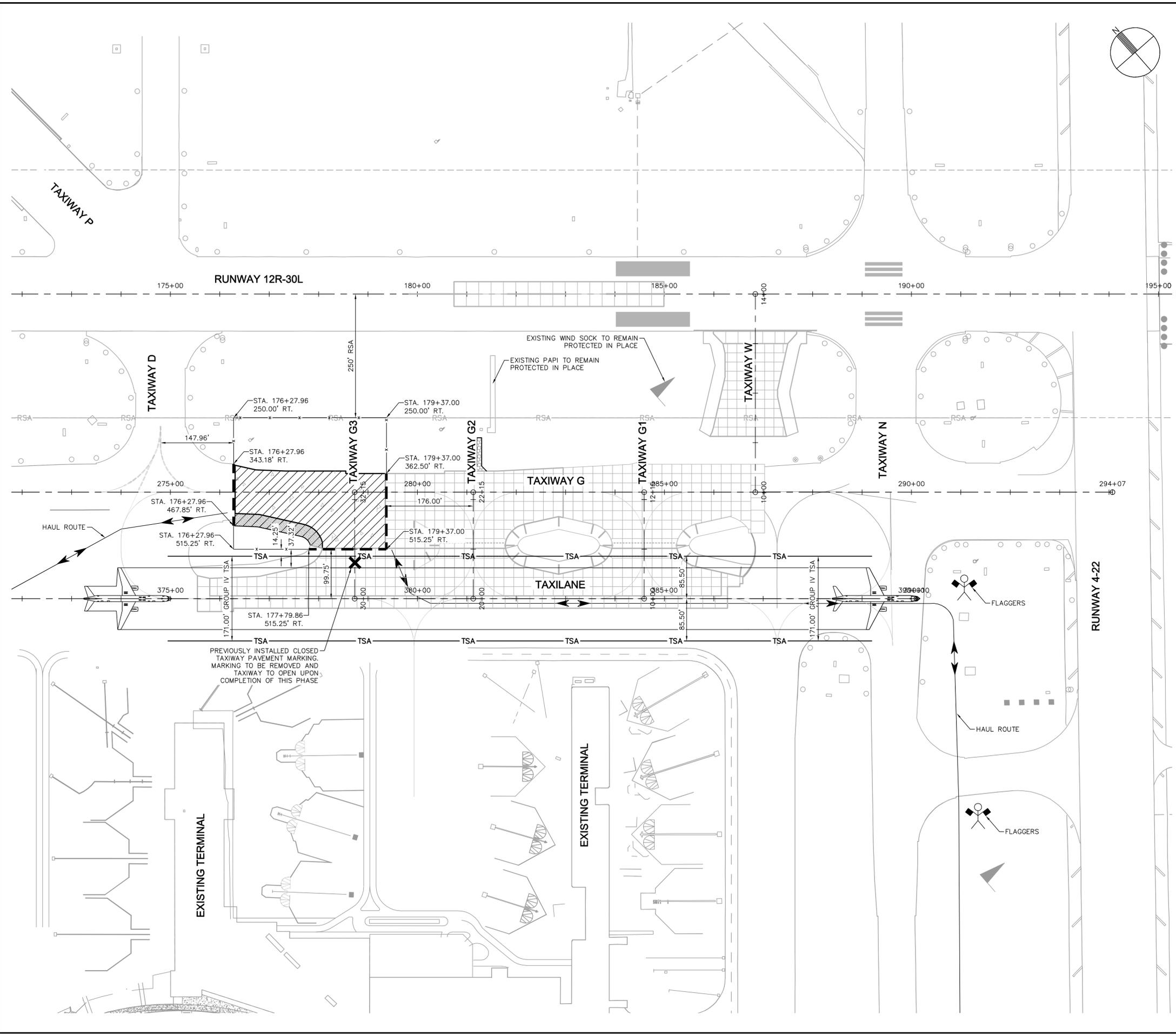
- PHASE 4
- PORTLAND CEMENT CONCRETE PAVEMENT
- ASPHALT CONCRETE SHOULDER PAVEMENT
- SAFETY FENCE
- LOW PROFILE BARRICADES
- HAUL ROUTE
- FLAGGER (TO BE SUPPLIED BY CONTRACTOR)
- WING WALKER (TO BE SUPPLIED BY AIRLINES)
- CLOSED RUNWAY/TAXIWAY PAVEMENT MARKING

GENERAL PHASE NOTES

1. A SEPARATE NTP WILL BE ISSUED FOR EACH PHASE.
2. ACCESS TO THE WORK AREA IS SHOWN ON THE OVERALL PLAN SHEETS.
3. EXISTING AIRFIELD LIGHTING CIRCUITS MUST REMAIN IN SERVICE FOR ALL AREAS OPEN TO AIRCRAFT DURING ALL HOURS OF DARKNESS AND DURING INSTRUMENT METEOROLOGICAL CONDITIONS (BELOW 1000' CEILING OR LESS THAN 3 MILES OF VISIBILITY) UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE AIRPORT. NO DIRECT PAY FOR JUMPERS. CURRENT WEATHER CONDITIONS FOR THE AIRPORT MAY BE OBTAINED BY CALLING (210) 805-5583.
4. THE CONTRACTOR SHALL REMOVE OR COVER ALL AIRFIELD GUIDANCE SIGNS FOR RUNWAYS AND TAXIWAYS THAT ARE CLOSED DURING EACH PHASE (NO DIRECT PAY).
5. THE CONTRACTOR SHALL REMOVE ALL TAXIWAY CENTERLINES LEADING INTO THE CONSTRUCTION AREA PRIOR TO THE COMMENCEMENT OF EACH PHASE (PAVEMENT MARKING REMOVAL) (NPI).

ALTERNATE 4 PHASE 4 NOTES

1. PHASE 4 CONSISTS OF THE NORTHWEST PORTION OF WORK ON TAXIWAY G AND THE NORTH PORTION OF TAXIWAY G3. STATION/OFFSET COORDINATES ARE TO PROVIDE THE GENERAL LOCATION OF THIS PHASE. REFER TO GEOMETRIC AND DEMO PLANS FOR CONSTRUCTION LIMITS.
2. FENCING AND BARRICADES MUST BE PLACED 250 FEET (OR MORE) FROM THE CENTERLINE OF RUNWAY 12R/30L, 85.5 FEET (OR MORE) FROM THE CENTERLINE OF THE TAXILANE, 85.5 FEET (OR MORE) FROM THE CENTERLINE OF THE ACTIVE PORTION TAXIWAY G, 85.5 FEET (OR MORE) FROM THE CENTERLINE OF TAXIWAY G2, AND 85.5 FEET (OR MORE) FROM THE CENTERLINE OF TAXIWAY N.
3. THIS PORTION OF TAXIWAY G BETWEEN TAXIWAY D AND TAXIWAY G2 WILL BE CLOSED TO AIRCRAFT OPERATIONS UNDER THIS PHASE OF CONSTRUCTION.
4. TAXIWAY N, TAXIWAY D, TAXIWAY G1, TAXIWAY G2, AND THE TAXILANE WILL REMAIN OPEN TO GROUP IV AND LOWER AIRCRAFT.
5. A PORTION OF TAXIWAY G (FROM TAXIWAY G2 TO TAXIWAY N) WILL REMAIN OPEN TO GROUP IV AND LOWER AIRCRAFT.
6. TAXIWAY G AND TAXIWAY G3 WILL BE RE-OPENED TO AIRCRAFT OPERATIONS UPON COMPLETION OF THIS PHASE.
7. ANY WORK REQUIRED TO BE COMPLETED WITHIN A TAXILANE/TAXIWAY SAFETY AREA WILL REQUIRE A TEMPORARY CLOSURE OF THE TAXILANE/TAXIWAY UNTIL THE WORK WITH THE SAFETY AREA IS COMPLETE.
8. THE CONTRACTOR SHALL PROTECT IN PLACE THE PAPI AND THE WIND SOCK. ANY DAMAGE TO THE EQUIPMENT SHALL BE REPORTED IMMEDIATELY TO THE AIRPORT. THE CONTRACTOR MUST ALLOW ACCESS TO THESE FACILITIES BY THE FAA OR AIRPORT PERSONNEL FOR MAINTENANCE.



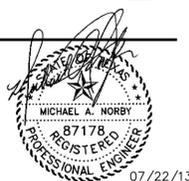
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TEXAS REGISTERED FIRM,
NO. 928

**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



MARK	DATE	DESCRIPTION

ISSUE: -

PROJECT NO: 33-00178 and 33-00193
FILE NAME: _33-00193-R1CP-405
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**CONSTRUCTION
PHASING PLAN
ALT 5
PHASE 5A & 5B**

LEGEND:

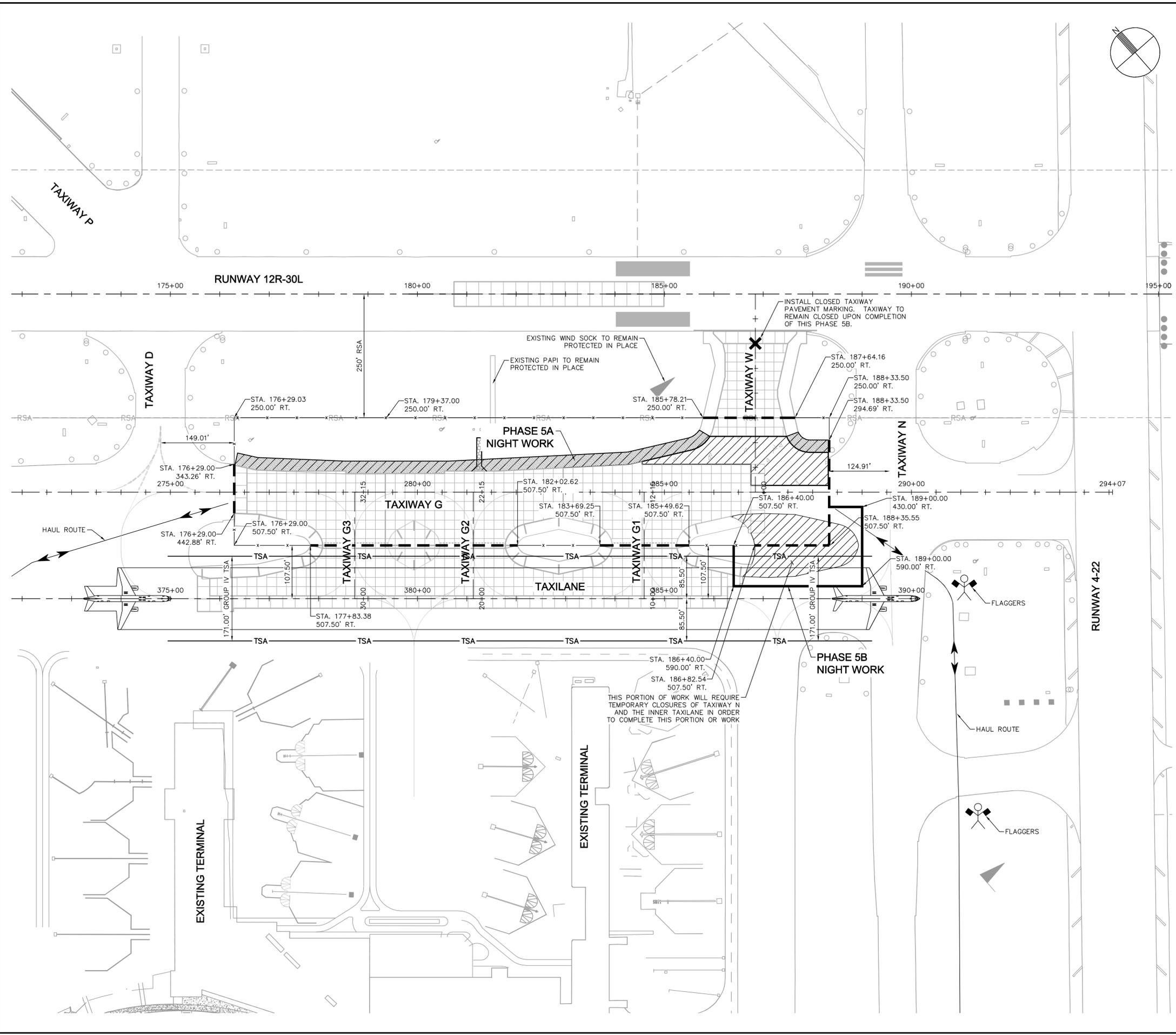
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- PORTLAND CEMENT CONCRETE PAVEMENT
- ASPHALT CONCRETE SHOULDER PAVEMENT
- SAFETY FENCE
- LOW PROFILE BARRICADES
- HAUL ROUTE
- FLAGGER (TO BE SUPPLIED BY CONTRACTOR)
- WING WALKER (TO BE SUPPLIED BY AIRLINES)
- CLOSED RUNWAY/TAXIWAY PAVEMENT MARKING

GENERAL PHASE NOTES

1. A SEPARATE NTP WILL BE ISSUED FOR EACH PHASE.
2. ACCESS TO THE WORK AREA IS SHOWN ON THE OVERALL PLAN SHEETS.
3. EXISTING AIRFIELD LIGHTING CIRCUITS MUST REMAIN IN SERVICE FOR ALL AREAS OPEN TO AIRCRAFT DURING ALL HOURS OF DARKNESS AND DURING INSTRUMENT METEOROLOGICAL CONDITIONS (BELOW 1000' CEILING OR LESS THAN 3 MILES OF VISIBILITY) UNLESS PRIOR APPROVAL HAS BEEN OBTAINED FROM THE AIRPORT. NO DIRECT PAY FOR JUMPERS. CURRENT WEATHER CONDITIONS FOR THE AIRPORT MAY BE OBTAINED BY CALLING (210) 805-5583.
4. THE CONTRACTOR SHALL REMOVE OR COVER ALL AIRFIELD GUIDANCE SIGNS FOR RUNWAYS AND TAXIWAYS THAT ARE CLOSED DURING EACH PHASE (NO DIRECT PAY).
5. THE CONTRACTOR SHALL REMOVE ALL TAXIWAY CENTERLINES LEADING INTO THE CONSTRUCTION AREA PRIOR TO THE COMMENCEMENT OF EACH PHASE (PAVEMENT MARKING REMOVAL) (NPI).

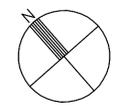
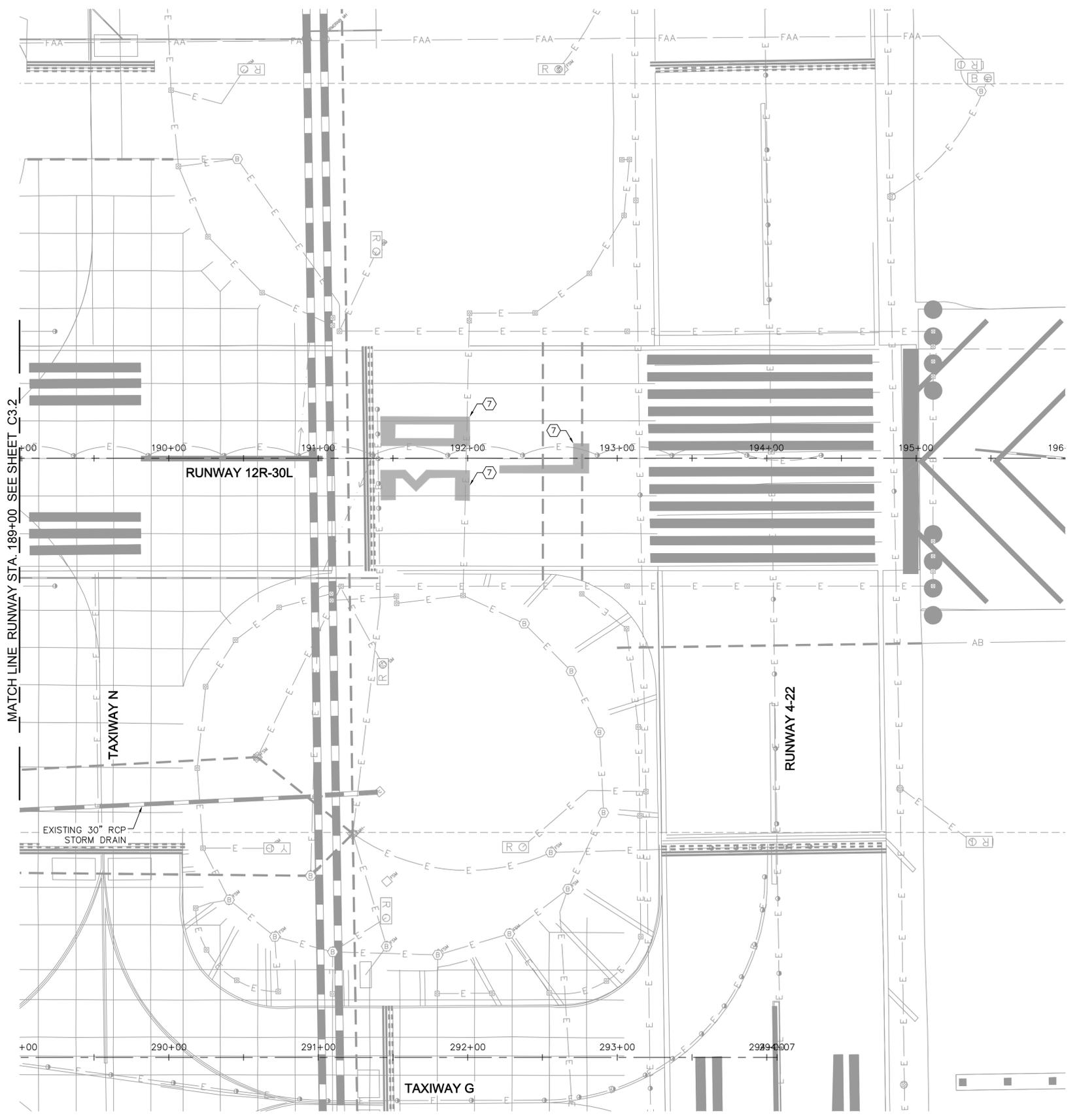
ALTERNATE 5 PHASE 5 NOTES

1. PHASE 5 CONSISTS OF THE NORTH SHOULDER OF TAXIWAY G, THE AREA CONNECTING TAXIWAY W AND TAXIWAY G, AND THE MARKING OF THE INFIELD AREA ADJACENT TAXIWAY G AND TAXIWAY N. STATION/OFFSET COORDINATES ARE TO PROVIDE THE GENERAL LOCATION OF THIS PHASE. REFER TO GEOMETRIC AND DEMO PLANS FOR CONSTRUCTION LIMITS.
2. FENCING AND BARRICADES MUST BE PLACED 250 FEET (OR MORE) FROM THE CENTERLINE OF RUNWAY 12R/30L, 85.5 FEET (OR MORE) FROM THE CENTERLINE OF THE TAXILANE, AND 85.5 FEET (OR MORE) FROM THE CENTERLINE OF TAXIWAY N.
3. TAXIWAY G1, TAXIWAY G2, TAXIWAY G3 AND THE PORTION OF TAXIWAY G BETWEEN TAXIWAY D AND TAXIWAY N WILL BE CLOSED TO AIRCRAFT OPERATIONS UNDER THIS PHASE OF CONSTRUCTION.
4. TAXIWAY N, TAXIWAY D, AND THE TAXILANE WILL REMAIN OPEN TO GROUP IV AND LOWER AIRCRAFT.
5. TAXIWAY G, TAXIWAY G1, TAXIWAY G2, AND TAXIWAY G3 WILL BE RE-OPENED TO AIRCRAFT OPERATIONS UPON COMPLETION OF THIS PHASE.
6. ANY WORK REQUIRED TO BE COMPLETED WITHIN A TAXILANE/TAXIWAY SAFETY AREA WILL REQUIRE A TEMPORARY CLOSURE OF THE TAXILANE/TAXIWAY UNTIL THE WORK WITH THE SAFETY AREA IS COMPLETE.
7. THE CONTRACTOR SHALL PROTECT IN PLACE THE PAPI AND THE WIND SOCK. ANY DAMAGE TO THE EQUIPMENT SHALL BE REPORTED IMMEDIATELY TO THE AIRPORT. THE CONTRACTOR MUST ALLOW ACCESS TO THESE FACILITIES BY THE FAA OR AIRPORT PERSONNEL FOR MAINTENANCE.



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LEGEND

-  PORTLAND CEMENT CONCRETE PAVEMENT REMOVAL
-  CLEAR AND GRUB (P-101)

GENERAL NOTES

1. EXISTING UTILITY INFORMATION SHOWN ON THE PLANS CONCERNING THE TYPE, SIZE, AND LOCATION WERE COMPILED BASED ON THE UTILITY RECORDS AVAILABLE TO THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A UTILITY LOCATOR AND VERIFY THE ACTUAL LOCATION OF UTILITIES PRIOR TO CONSTRUCTION. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING UTILITIES IN PLACE UNLESS NOTED OR SPECIFIED OTHERWISE. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. UTILITIES INTERFERING WITH CONSTRUCTION SHALL BE RESET OR RELOCATED BY THE UTILITY COMPANY CONCERNED UNLESS NOTED OTHERWISE.
2. REFER TO SHEET C1.12 FOR HORIZONTAL AND VERTICAL CONTROL.
3. REFER TO ELECTRICAL PLANS (SERIES E) FOR ELECTRICAL DEMOLITION.

CONSTRUCTION NOTES

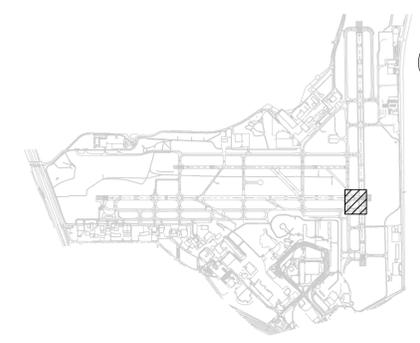
- ① SAWCUT EXISTING PCC PAVEMENT - FULL DEPTH
- ② REMOVE AND DISPOSE OF EXISTING PCC PAVEMENT OFFSITE
- ③ REMOVE AND DISPOSE OF EXISTING STORM DRAIN PIPE, LIMITS OF REMOVAL PER PLAN
- ④ REMOVE AND DISPOSE OF EXISTING DRAINAGE STRUCTURE
- ⑤ REFER TO ELECTRICAL PLANS (SERIES E)
- ⑥ REMOVE AND DISPOSE OF EXISTING AC PAVEMENT OFFSITE
- ⑦ PAVEMENT MARKING TO BE OBLITERATED

CITY OF
SAN ANTONIO
AVIATION
DEPARTMENT



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NO. 928

**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



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 FILE NAME: 33-00193-R1CD-101
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**DEMOLITION PLAN
BASE BID**



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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



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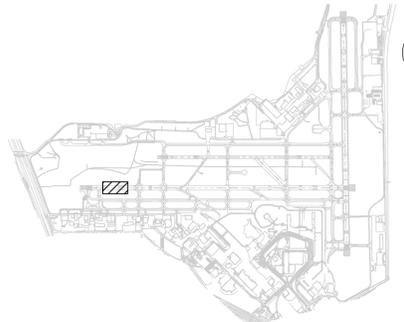
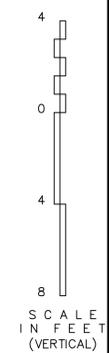
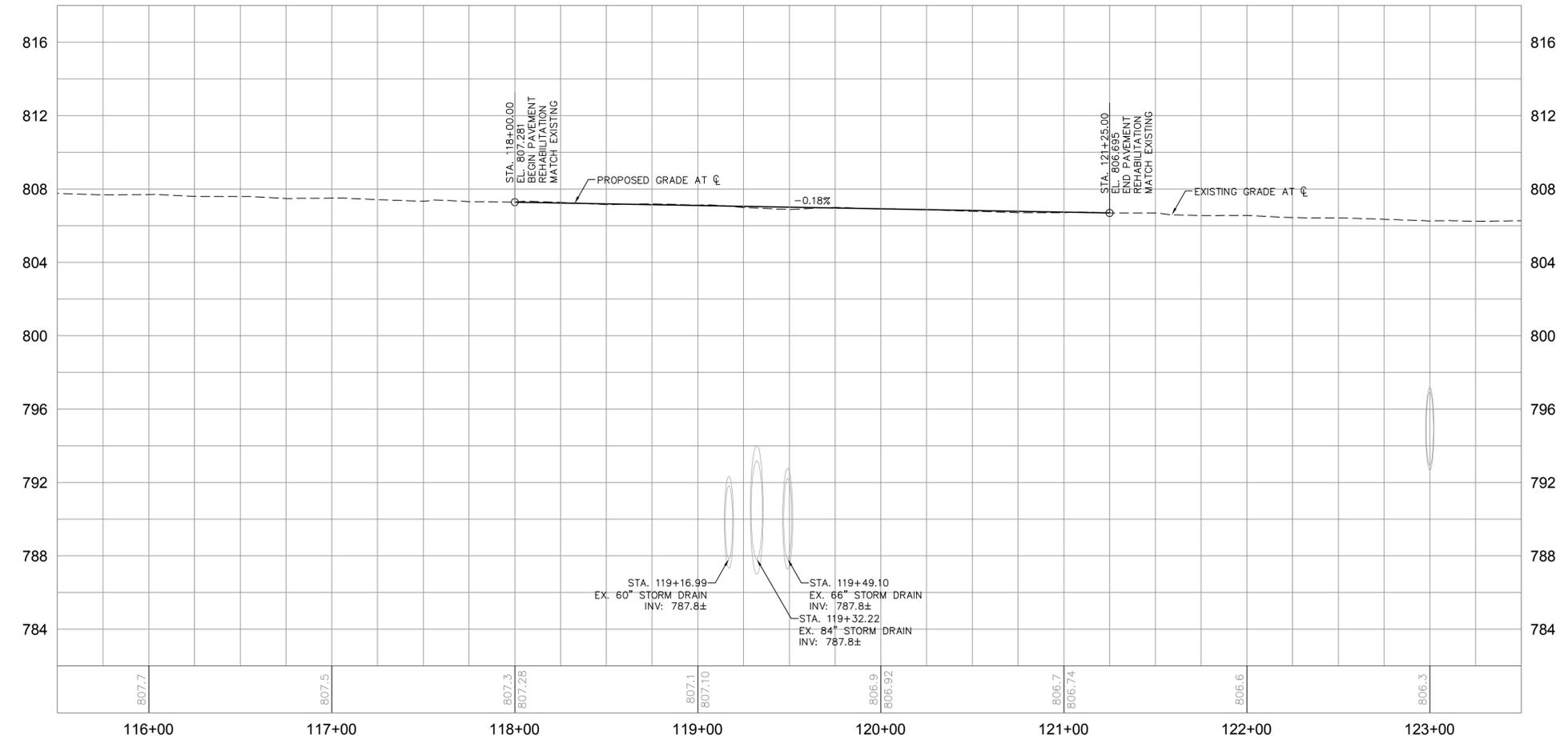
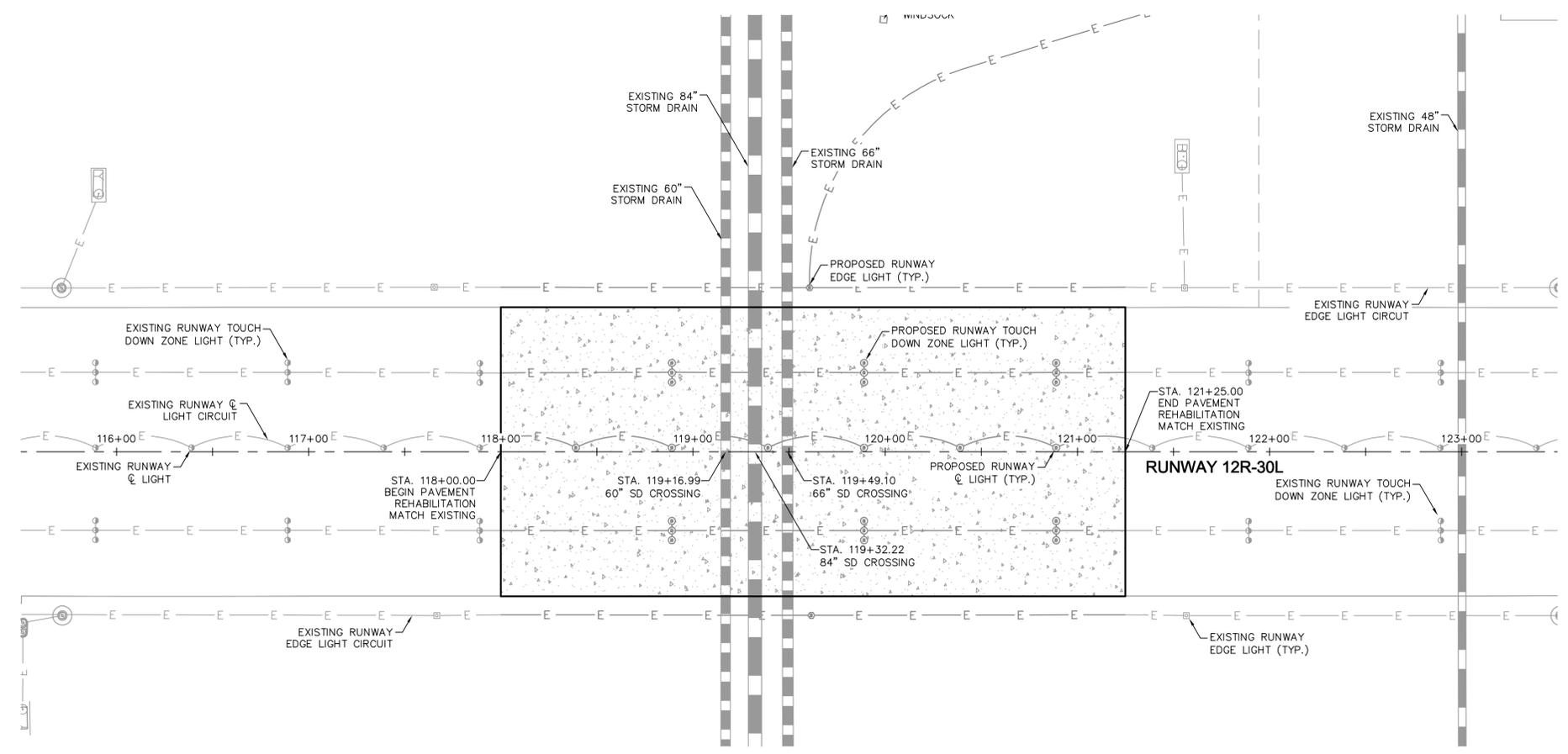
**PLAN AND PROFILE
RUNWAY 12R END
BASE BID**

LEGEND

- PORTLAND CEMENT CONCRETE PAVEMENT
- PORTLAND CEMENT CONCRETE TRANSITION
- ASPHALT CONCRETE SHOULDER PAVEMENT
- ASPHALT CONCRETE PAPI ROAD PAVEMENT

GENERAL NOTES

1. ALL STATIONS AND OFFSETS REFER TO THE APPROPRIATE RUNWAY OR TAXIWAY STATIONING.
2. REFER TO ELECTRICAL PLANS (SERIES E) FOR AIRFIELD LIGHTING, SIGNING, AND ELECTRICAL FEATURES.



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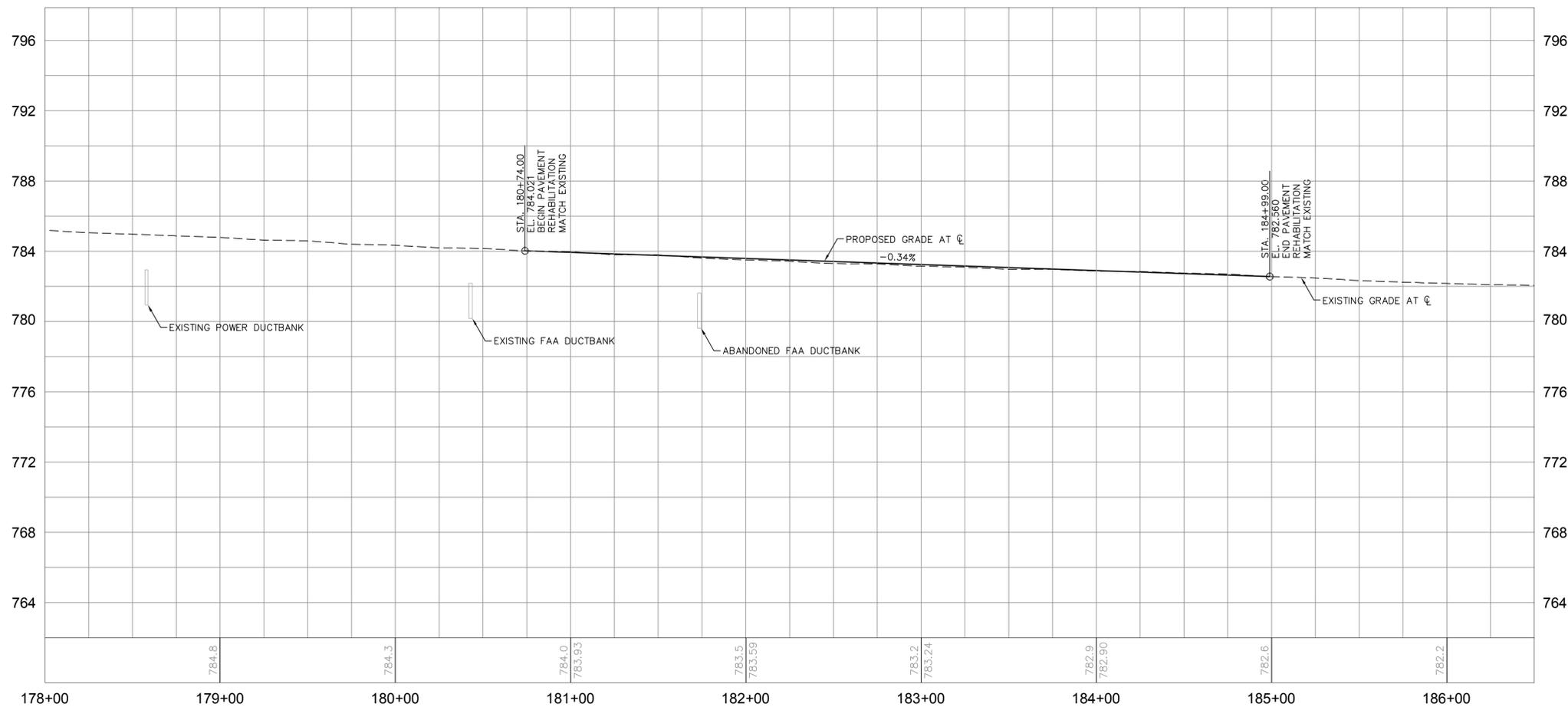
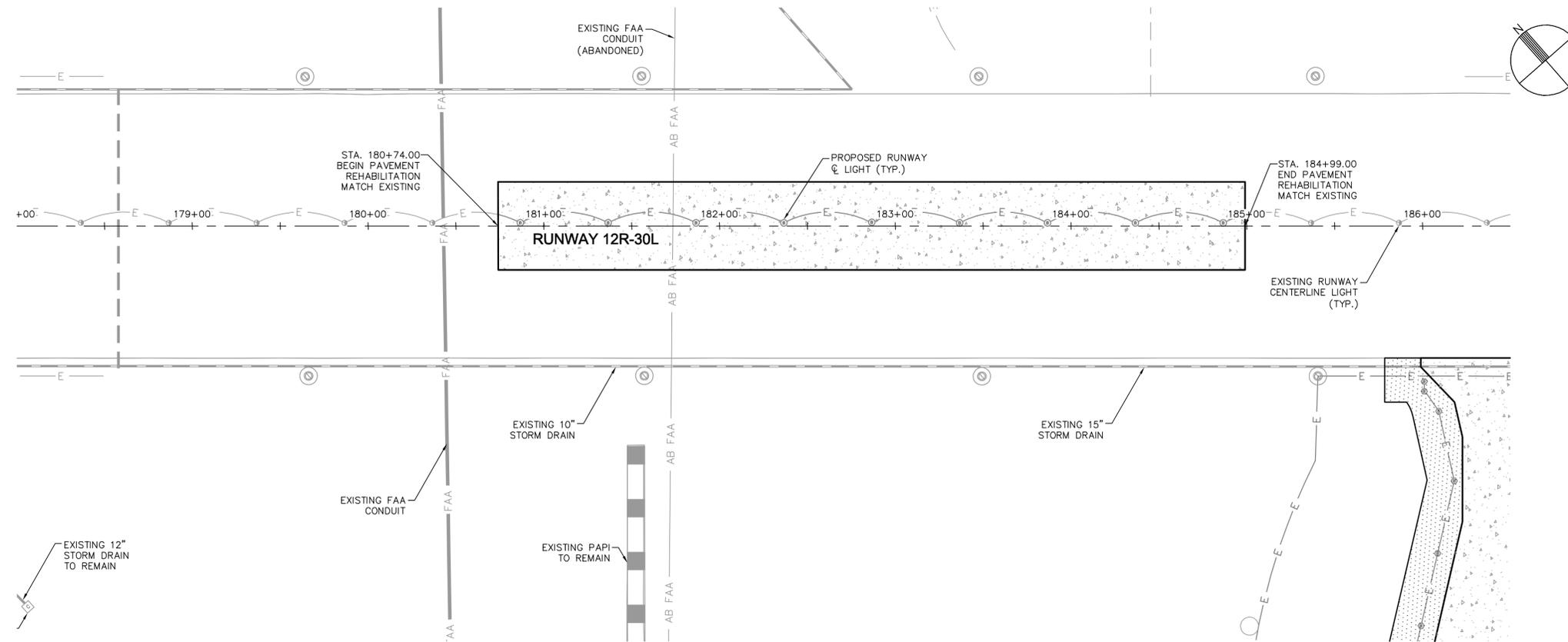
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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**

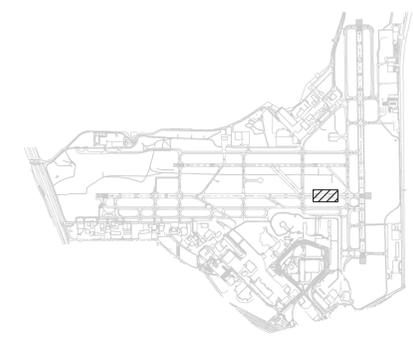


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**PLAN AND PROFILE
RUNWAY 30L END
BASE BID**



SCALE
IN FEET
(VERTICAL)



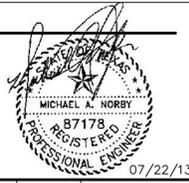
SCALE
IN FEET

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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**

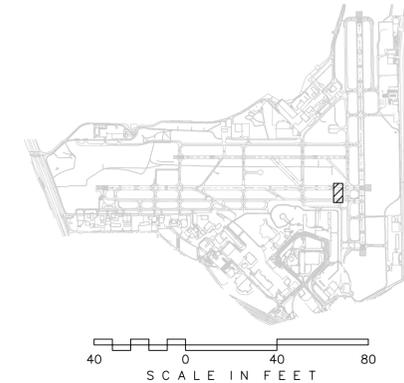
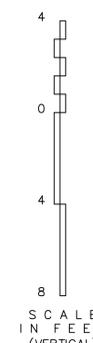
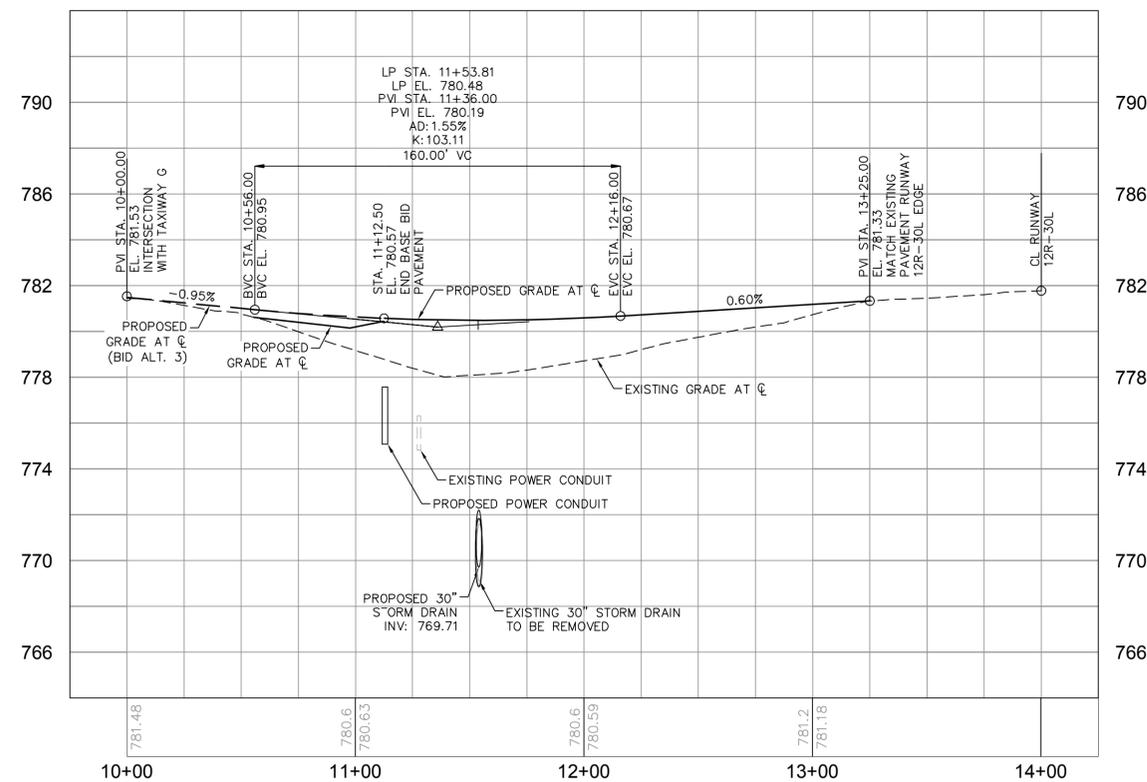
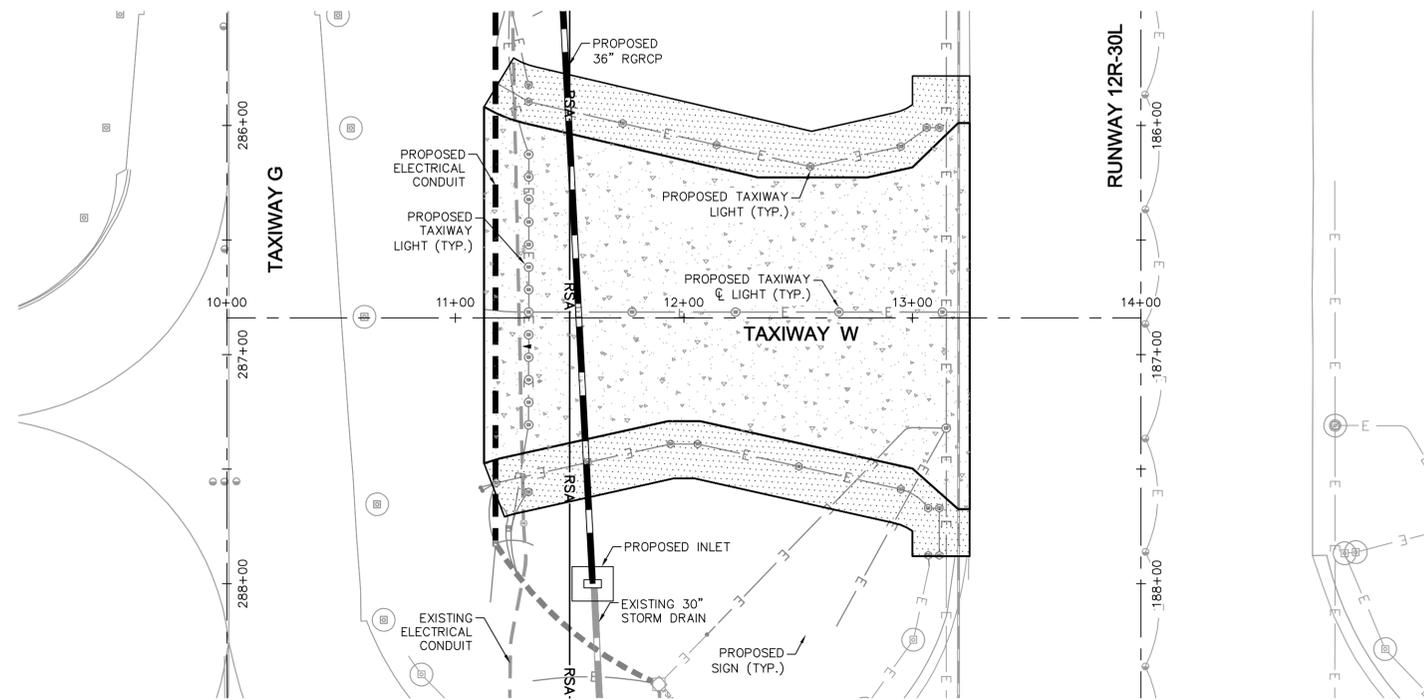
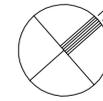


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SHEET TITLE:

**PLAN AND PROFILE
TAXIWAY W
BASE BID**

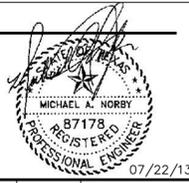


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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXWAY
IMPROVEMENTS (PACKAGE 1)**



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**PAVEMENT JOINT
LAYOUT PLAN
BASE BID**

LEGEND

- PORTLAND CEMENT CONCRETE PAVEMENT
- PORTLAND CEMENT CONCRETE TRANSITION
- ASPHALT CONCRETE SHOULDER PAVEMENT
- ASPHALT CONCRETE PAPI ROAD PAVEMENT

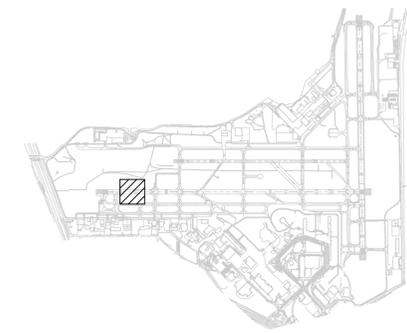
- (A) JOINT TYPE A, REFER TO SHEET C7.3 FOR PAVEMENT JOINT DETAILS
- (A1) JOINT TYPE A1, REFER TO SHEET C7.3 FOR PAVEMENT JOINT DETAILS
- (C) JOINT TYPE C, REFER TO SHEET C7.3 FOR PAVEMENT JOINT DETAILS
- (E) JOINT TYPE E, REFER TO SHEET C7.3 FOR PAVEMENT JOINT DETAILS
- (4) EDGE SEAL TYPE 4, REFER TO SHEET C7.3 FOR PAVEMENT JOINT DETAILS
- (9) JOINT TYPE 9, REFER TO SHEET C7.3 FOR PAVEMENT JOINT DETAILS

GENERAL NOTES

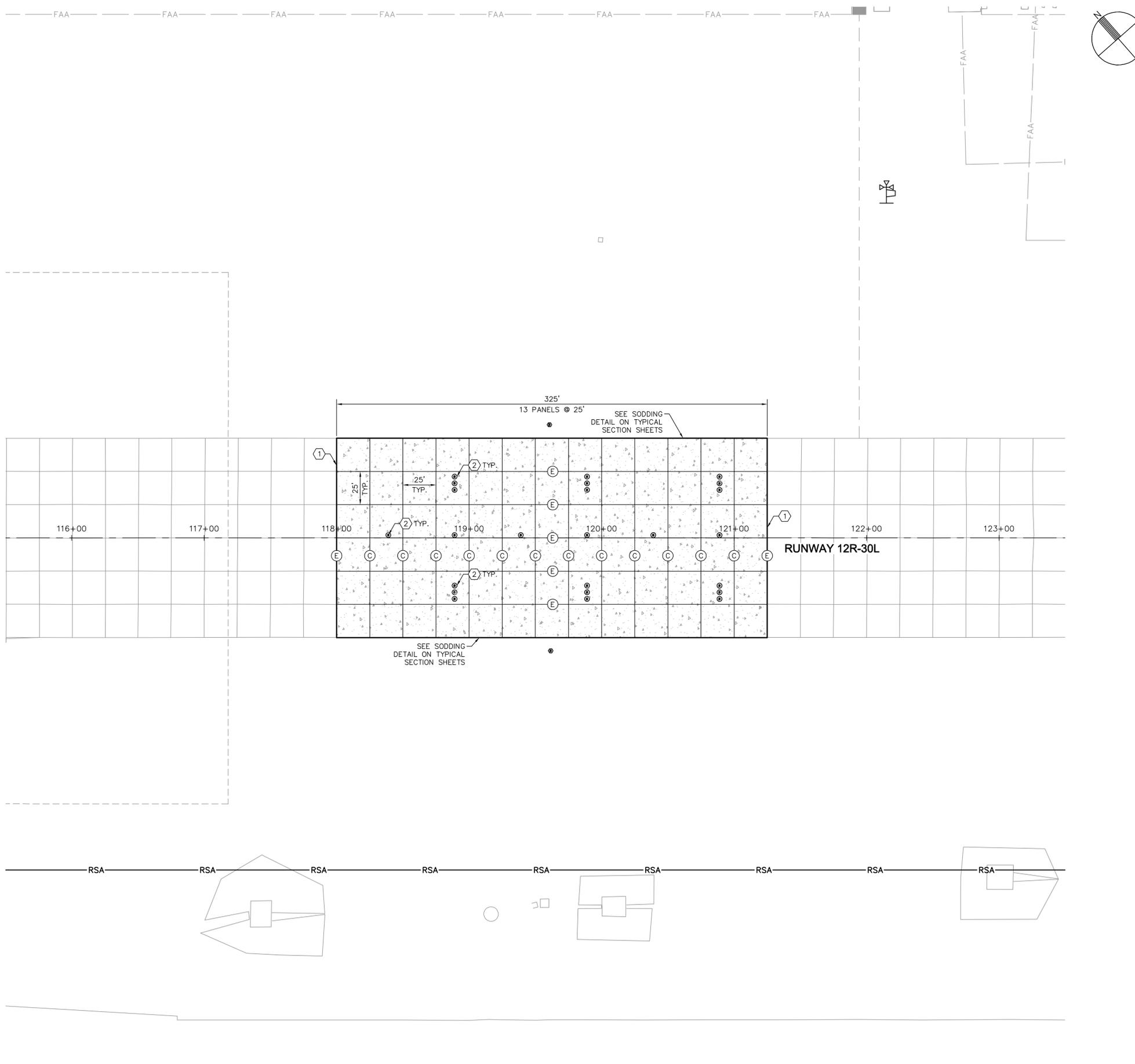
1. REFER TO SHEET C1.12 FOR HORIZONTAL AND VERTICAL CONTROL.
2. TYPE C AND TYPE E JOINTS MAY BE INTERCHANGED WITH APPROVAL FROM THE ENGINEER.
3. PORTLAND CEMENT CONCRETE PAVEMENT (P-501) JOINTS SHALL NOT VARY MORE THAN 1/2-INCH FROM THEIR DESIGNATED POSITION AND SHALL BE TRUE TO LINE WITH NOT MORE THAN 1/4-INCH VARIATION IN 10-FEET. ANY JOINT(S) THAT ARE NOT PARALLEL OR PERPENDICULAR, OTHER THAN IRREGULAR SLABS, SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR WITH NO ADDITIONAL COST TO THE AIRPORT.
4. ALL SAWCUT DEBRIS FROM THE WIDENING OF THE JOINTS SHALL BE REMOVED FROM THE CONCRETE SURFACE IMMEDIATELY AFTER SAWCUTTING BY VACUUMING THE SLURRY.
5. DOWEL BARS SHALL NOT VARY IN ALIGNMENT BY MORE THAN TWO-PERCENT (2%) OR 0.40" IN EACH PLANE. MISALIGNED DOWEL BARS BY MORE THAN THE SPECIFIED TOLERANCE SHALL REQUIRE THE PCCP TO BE REMOVED ON EACH SIDE OF THE DOWEL BARS AT NO ADDITIONAL COST TO THE AIRPORT.
6. CONTRACTOR TO COORDINATE WITH THE ENGINEER FOR JOINTING AROUND ELEC./COMM. MANHOLES THAT ARE TO REMAIN IN PLACE.
7. ALL PCCP PANELS ARE REINFORCED PER DETAIL 6, SHEET C7.3

CONSTRUCTION NOTES

- (1) SAWCUT AND MATCH EXISTING JOINT SPACING AND ELEVATION
- (2) REFER TO ELECTRICAL PLANS (SERIES E)



40 0 40 80
SCALE IN FEET



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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



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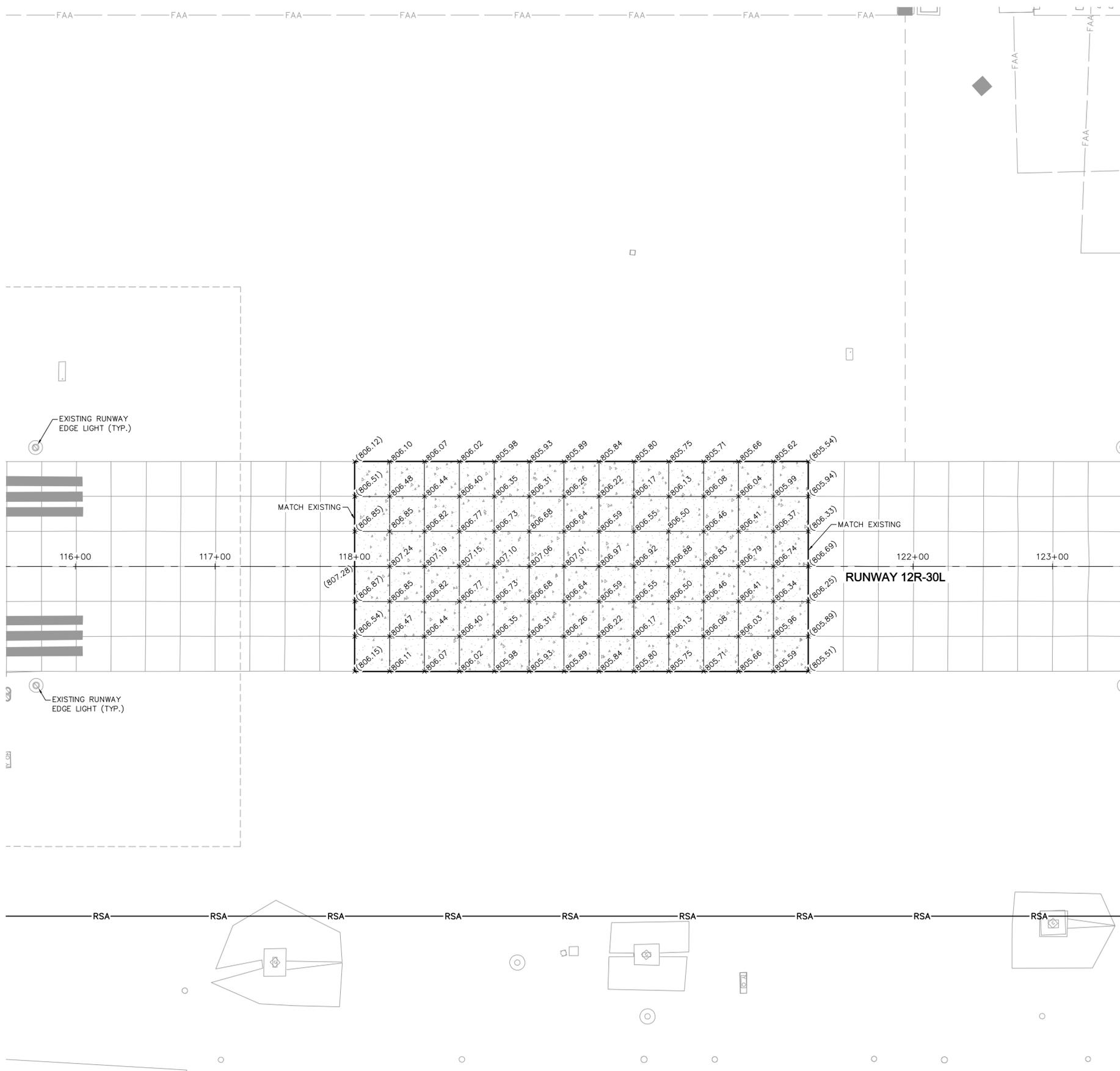
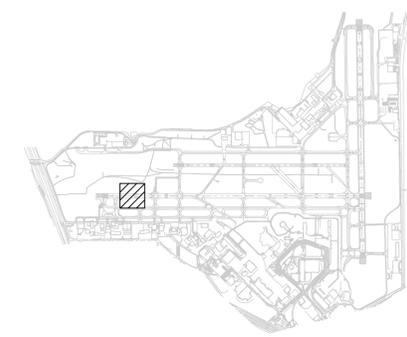
**PAVING ELEVATION
PLAN
BASE BID**

LEGEND

- PORTLAND CEMENT CONCRETE PAVEMENT
- PORTLAND CEMENT CONCRETE TRANSITION
- ASPHALT CONCRETE SHOULDER PAVEMENT
- ASPHALT CONCRETE PAPI ROAD PAVEMENT
- PROPOSED SPOT ELEVATION

GENERAL NOTES

1. REFER TO SHEET C1.12 FOR HORIZONTAL AND VERTICAL CONTROL.
2. PAVEMENT ELEVATIONS SHOWN ARE BASED ON JOINT LAYOUT, REFER TO PAVEMENT JOINT LAYOUT PLANS (SERIES C7).
3. CONTRACTOR SHALL MATCH EXISTING PAVEMENT ELEVATIONS AS SHOWN ON PLAN. IF EXISTING ELEVATIONS DIFFER FROM THOSE SHOWN, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY.
4. REFER TO ELECTRICAL PLANS (SERIES E) FOR AIRFIELD LIGHTING, SIGNING, AND ELECTRICAL FEATURES.



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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



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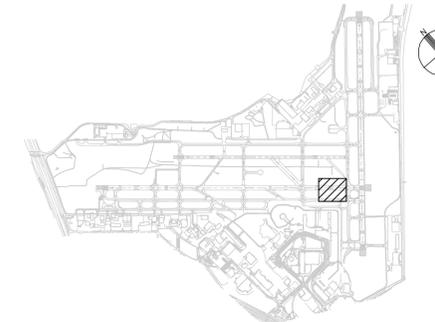
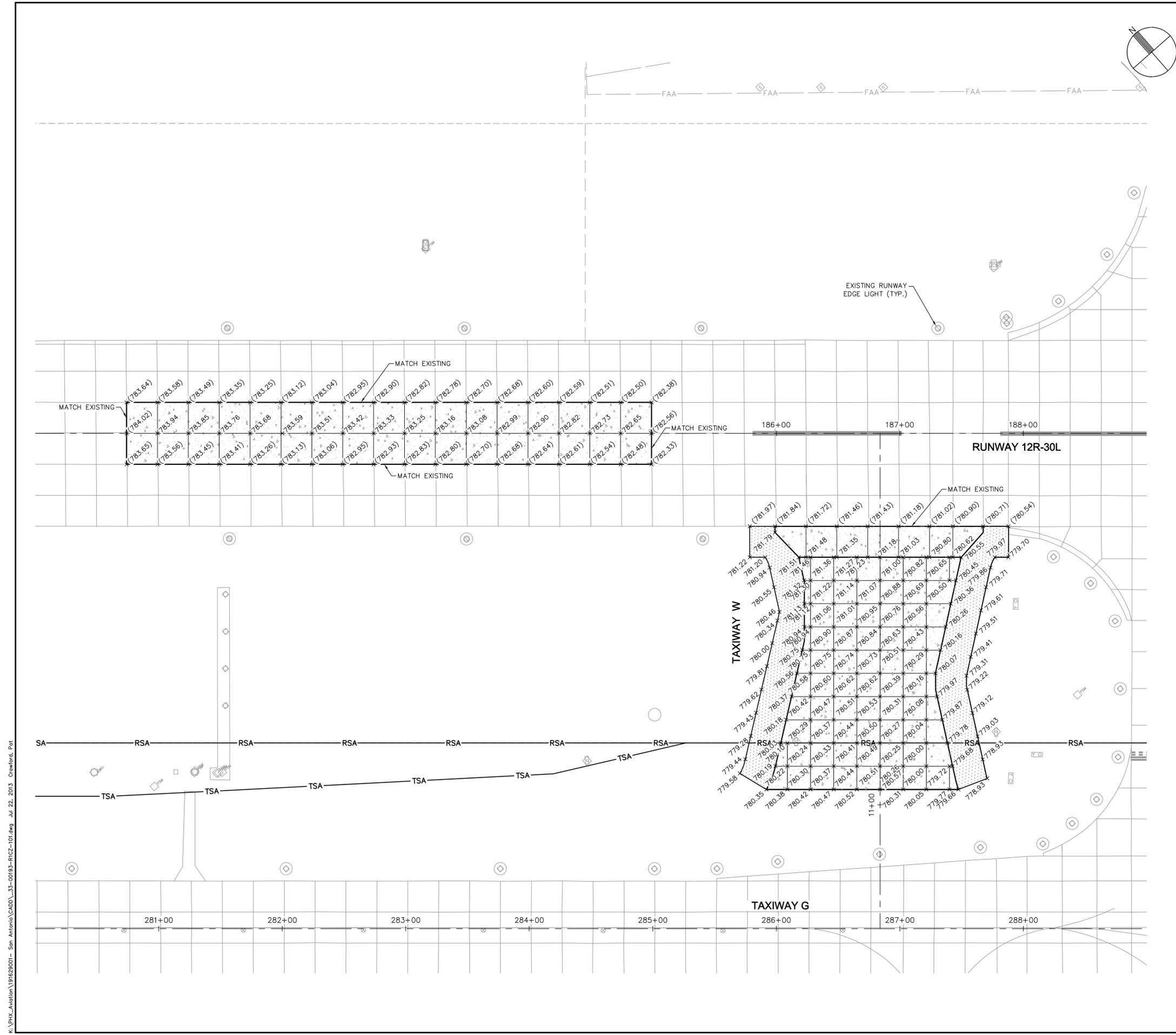
**PAVING ELEVATION
PLAN
BASE BID**

LEGEND

- PORTLAND CEMENT CONCRETE PAVEMENT
- PORTLAND CEMENT CONCRETE TRANSITION
- ASPHALT CONCRETE SHOULDER PAVEMENT
- ASPHALT CONCRETE PAPI ROAD PAVEMENT
- PROPOSED SPOT ELEVATION

GENERAL NOTES

1. REFER TO SHEET C1.12 FOR HORIZONTAL AND VERTICAL CONTROL.
2. PAVEMENT ELEVATIONS SHOWN ARE BASED ON JOINT LAYOUT, REFER TO PAVEMENT JOINT LAYOUT PLANS (SERIES C7).
3. CONTRACTOR SHALL MATCH EXISTING PAVEMENT ELEVATIONS AS SHOWN ON PLAN. IF EXISTING ELEVATIONS DIFFER FROM THOSE SHOWN, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY.
4. REFER TO ELECTRICAL PLANS (SERIES E) FOR AIRFIELD LIGHTING, SIGNING, AND ELECTRICAL FEATURES.



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**RUNWAY 12R-30L
REHABILITATION AND
TERMINAL AREA TAXIWAY
IMPROVEMENTS (PACKAGE 1)**



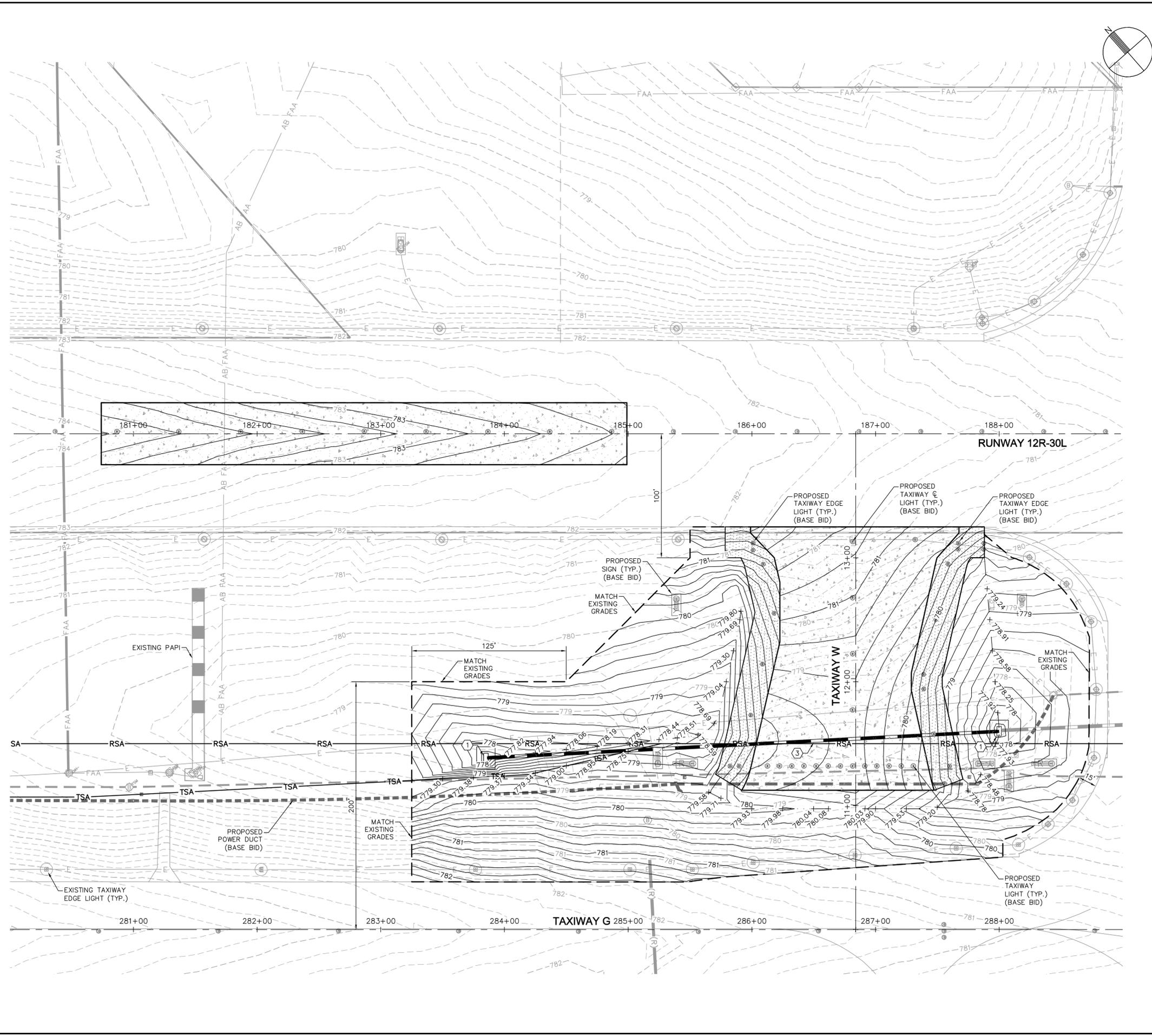
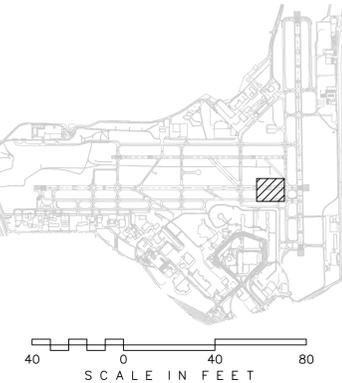
MARK	DATE	DESCRIPTION

**GRADING AND
DRAINAGE PLAN
BASE BID**

- LEGEND**
- PORTLAND CEMENT CONCRETE PAVEMENT
 - PORTLAND CEMENT CONCRETE TRANSITION
 - ASPHALT CONCRETE SHOULDER PAVEMENT
 - ASPHALT CONCRETE PAPI ROAD PAVEMENT
 - GRADING LIMITS
 - PROPOSED SPOT ELEVATION

- GENERAL NOTES**
1. EXISTING UTILITIES ARE SHOWN IN APPROXIMATE LOCATIONS ONLY. CONTRACTOR SHALL VERIFY EXACT LOCATIONS OF ALL UNDERGROUND UTILITIES PRIOR TO COMMENCING WORK IN ANY AREA.
 2. ALL STATIONS AND OFFSETS REFER TO THE APPROPRIATE RUNWAY OR TAXIWAY STATIONING.
 3. DESIGN CONTOURS REPRESENT FINISHED GRADE (PAVEMENT AND GRADING). CONTOURS AND PERCENT OF SLOPE ARE APPROXIMATE AND ARE SHOWN AS SUPPLEMENTAL INFORMATION ONLY.
 4. REFER TO ELECTRICAL PLANS (SERIES E) FOR AIRFIELD LIGHTING, SIGNING, AND ELECTRICAL FEATURES.

- CONSTRUCTION NOTES**
- ① INSTALL CATCH BASIN PER DETAILS (SERIES C9)
 - ② INSTALL STORM DRAIN MANHOLE PER DETAILS (SERIES C9)
 - ③ INSTALL RGRCP PER DETAILS (SERIES C9)



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